

ARCHITECTURAL DESIGN GUIDELINES

for

TRACE TOWN CENTER



Amended September 30, 2019

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Note: All text and information boxed and highlighted in red are direct references to the Master Plan and Planned Development District Standards for Trace dated October 21, 2015.

Section 1: Development Themes

1.1 Identity

Trace Town Center is in the heart of the Texas Blackland Prairie. The master planned community is bounded by and has immediate access to I-35 and Posey Road. The development's main collector road, Van Horn Trace, and sub collector roads has direct connection to I-35 to support its *town center* identity and visibility that will encourage its continued success.

1.2 Connectivity

Trace Town Center is an integral component of the Trace community and emphasizes the relationship of the surrounding residential areas. The organization and design of the streets are designed to provide easy access for pedestrians and bicycles, making it a short commute to retail establishments and other services from all homes within the community.

1.3 Walkability

The main thoroughfares are designed with landscape and tree cover to provide shade for pedestrians and building facades will be articulated with awnings and porticos to encourage outdoor dining venues.

1.4 Convenience

The development is programmed and configured to be convenient for residents and employees alike. The street grid is designed to promote easy access and mobility for motorists as well as pedestrians.

1.5 Diversity

With its mix of shops, offices, near to apartments and homes, it is also a wonderful place to live and do business.

1.6 Authenticity

The Trace Commercial Development represents the best of San Marcos. Local Retailers, restaurateurs, and homegrown businesses are encouraged within the district, giving it a distinctive San Marcos character.

Section 2: Development Strategy:

The development strategy calls for the creation of diverse mixed-use district with the following key components:

- Specialty retail will line the main frontage of the development and will act as a welcoming gateway to the community.
- In addition to the retail establishments, there are a number of locations for hotels to support travel commerce for the development and the City of San Marcos.
- The main body of the development will be home to a range of single story and multi-story commercial buildings that may provide services that range from medical support to corporate entities.
- The development will be able to support a multi building campus arrangement that will strengthen the pre-defined themes of the area.
- Parking will be centralized within the development.
- Other uses can be allowed upon review and as permitted by the PDD and the San Marcos Zoning Ordinance.

Section 3: Building Types

Trace Town Center will be comprised of several building types, which include the following:

- Retail
 - Shopping Center
 - Neighborhood Commercial Services
 - Grocery Store
- Restaurants
- Hotels
- Office Buildings
 - One-story to three-stories typical
 - Buildings exceeding three-stories shall be allowed upon review and acceptance of the Master Developer and the City of San Marcos.
- Parking Garages
- Gas Stations

Note: Building use, size, and height shall not exceed parameters set forth by the San Marcos Building Ordinance.

Section 4: Base Criteria and Guidelines

The vision and themes outlined in this document is intended to help encourage the aesthetic and connectivity of commercial development with the surrounding neighborhood and the City of San Marcos as a whole. The final design and requirements for the community will involve further coordination between developers, users, tenants, and designers in accordance with the Planned Development District Standards (PDD) and the City of San Marcos Zoning Ordinance.

Section 5: Conceptual Master Plan



Note: This master plan conceptual in nature and is provided as a reference only. This site does not reflect final and approved roadways and other site elements.

5.1 Master Plan Alternate 1



Note: This master plan conceptual in nature and is provided as a reference only. This site does not reflect final and approved roadways and other site elements.

5.2 Master Plan Alternate 2



Note: This master plan is conceptual in nature and is provided as a reference only. This site does not reflect final and approved roadways and other site elements.

5.3 Master Plan Alternate 3



Note: This site plan is only conceptual in nature and is provided as a reference only. This site does not reflect final and approved roadways and other site elements.

Section 6: Development Sub-areas

6.1 I-35 Frontage (Highway)

The I-35 Frontage is the principal point of visibility and access for the Trace community and will serve as a gateway to the neighborhood and other services. This area will consist of the following:

- A continuous frontage of retail, restaurant, or pedestrian-oriented uses.
- Canopies and awnings and/or arcades and colonnades that provide cover and shade along face of the building.
- Retail uses are encouraged to be oriented away from the I-35 frontage and towards interior parking areas.
- Surface parking should be planned as outdoor “rooms”, spatially defined by buildings, and with a significant tree canopy and surface level vegetation to mediate the harsh summer sun.



6.3.1 Orientation

- *Buildings should respond to the orientation of neighboring buildings within the commercial areas and to neighboring uses. i.e commercial to commercial or commercial to multi-family.*
- *Buildings may back up to I-35 provided they are screened and setback appropriately.*
- *Buildings may be oriented towards I-35 but should also be oriented to the Collector Roadway as shown on Illustration 40: Allowable Commercial Orientation.*

6.2 Van Horn Trace (Main Collector Roadway)

The Van Horn Trace frontage will stand as the transition from retail services to the residential development and will primarily be lined with a series of commercial buildings with ranging elevations and heights. The primary purpose of the main collector road is to encourage the connectivity between the services and places of work with the residents they serve.



6.3.1 Orientation

- *In no case shall the rear of a building back up to a Collector Roadway.*

6.3 Sub Collector Roadways

The sub collector roadways will serve as the main internal connection within the development and will therefore continue the pedestrian themes encouraged by the Van Horn Trace frontage.



6.4 Internal Drives

Internal drives will serve as access points for various maintenance and like services with the intent of screening these components from the main public ways. These services include, but are not limited to, access to parking garages, delivery operations, and placement of building support equipment.



6.5 Perimeter Creek Side Frontage

The pattern of roadways within the development will be designed to enhance visibility to the perimeter creek side frontage and to make it more visible to surrounding areas.



6.6 Multifamily and Commercial Pedestrian Access

Boundaries between commercial and multifamily zones will be designed to provide elements and services to promote a pedestrian friendly environment.



Section 7: Guidelines

Buildings and outdoor spaces will be designed to the following guidelines:

7.1 Exterior Open Space:

- Buildings are encouraged to provide paseos and courtyards that provide pedestrian linkages to public parking facilities and/or to other development activities.
- Plazas, courtyards, and other open spaces will be designed as an integral component of the development. Designed to promote the attractiveness of the subarea as people-oriented and pedestrian-friendly destination within the community.
- Such space should be designed to provide shade and cover for pedestrians, as well as opportunities for outdoor dining, information seating, street vending and performances, as well as small gatherings.
- A relatively unbroken continuity of open space will be created with strong linkages to existing parks or natural features. The pattern of roadways will be designed to enhance accessibility to greenways and to make it more visible to surround areas.

6.3.6 Exterior Site Design Requirements for Commercial Areas

These areas shall be enhanced through specific urban design regulations, including:

- *Pedestrian activities such as "al fresco" dining along the sidewalk should be promoted to enhance the character of the commercial area.*
- *Create pedestrian areas that encourages pedestrian activity while accommodating vehicular requirements.*
- *The use of pedestrian promenades characterized by outdoor plaza elements between commercial buildings should create unity, cohesiveness and a sense of space while promoting walkable pedestrian activity centers.*
- *Commercial building entries should incorporate pedestrian elements and amenities such as seating, lighting, and hardscape elements that help create or enhance special pedestrian areas.*
- *Detention Location and Design for Commercial Areas shall be held to the highest standard of the Stormwater Technical Manual, Appendix N, on file with the City of San Marcos.*

7.2 Ground Level Treatment:

- The street frontage will be designed with high bay storefronts, with a building floor-to-floor height of no less than 16 feet, and with window to wall transparency ratio on the ground floor of at least 60 percent.
- The primary entry to all uses must be oriented towards the highway or the adjacent sub-collector roadways at intervals of 50 feet.
- High quality accent materials should be employed along the storefronts of both the I-35 frontage and the adjacent sub-collector roadways. Such materials could include stone, wood, and metals. Reflective glass is prohibited.

6.3.2 Form & Scale

Ground Level Articulation: (All facades visible from public view)

- *Where there is a retail use, ground floor facades shall have arcades, display windows, entry areas, awnings, and other features no less than 50% of their entire length.*
- *Where there is an office use, ground floor facades shall have entry areas, awnings, and other features no less than 25% of their entire length.*

7.3 Materials Required

All buildings shall incorporate the building materials listed in Table below with the allowable percentages of such materials. One hundred percent (100%) masonry is required on all facades with the exception of the permitted secondary and accent materials. The percent calculations shall be based on total exterior walls, excluding openings for windows and doors.

Building Materials	Allowable Percentage
Primary Masonry	100% (unless secondary or accent materials are utilized then the primary masonry materials may be reduced to a minimum of 40%)
Clay brick	
Natural Stone	
Manufactured stone with a natural appearance	
Secondary Masonry	Maximum 60%
Concrete panels	
Stucco	

Accent Materials

Architectural concrete block/concrete masonry unit (CMU) Maximum 15%
Exterior Insulation and Finish Systems (EIFS)
Cementitious material installed as horizontal lap siding
Metal panels
Metal (for beams, lintels, trim elements, and ornaments)
Glass block
Tile
Other materials approved

6.3.5 Materials

- *Buildings shall be clad with glass, steel, architectural metals, and/or masonry materials such as decorative/textured split face concrete block, brick, stone, or cement stucco.*
- *Synthetic stucco materials, such as Exterior Insulation and Finish Systems (EIFS) and composite materials such as cementitious fiberboard (hardiplank), should be limited to no more than 20 percent of the total area of each façade, not including doors and windows.*
- *Permitted Materials for Roof Construction. The following materials are permitted for roof construction: Copper, Metal sheet roofing, Tile – Concrete or Clay, A Commercial Flat (built up) Roof and/or Composition Roof.*

7.4 Architectural Details

7.4.1 Architectural Elements

All buildings shall be designed to incorporate no less than four (4) of the architectural elements from the list below. Buildings or multitenant buildings over 50,000 square feet shall include no less than five (5) of the referenced architectural elements. Buildings or multitenant buildings over 100,000 square feet shall include no less than six (6) of the referenced architectural elements:

- Canopies, awnings, or porticos
- Arcades
- Pitched roof forms
- Arches
- Display windows
- Architectural details (such as tile work and moldings) integrated into the building façade
- Articulated ground floor levels or base
- Articulated cornice line

- A minimum of two building materials constituting a minimum of 15% of the total exterior walls, differentiated by texture, color, or material and may be a combination of primary and secondary masonry materials and accent materials; and
- Other architectural features approved by the Master Developer.

6.3.2 Form & Scale

Vertical Articulation: (All facades visible to public view)

- Architectural details employed in the three part façade shall include a minimum of three (3) of the following: water tables, string course, sign band, cornice, display windows, overhanging eaves, change in color, change in material, change in texture, or fenestration.
- Multi-Story buildings shall also employ a pattern of windows for the middle and upper section(s)".

7.4.2 Common Development

All buildings within a common development shall have similar architectural styles and materials as approved by the Master Developer.

7.4.3 Facade Finish

All nonresidential buildings shall be architecturally finished on all four sides with same materials, detailing, and features unless noted otherwise on other sections of this document.

7.4.4 Articulation Standards

Any primary façade shall include projections or recesses and vertical variations in the roof line in accordance with the horizontal and vertical articulation requirements set forth below.

- Horizontal Articulation
 - A building facade may not extend for a distance greater than three times its average height without a perpendicular offset of at least ten percent of such building height.
 - The total length of all façade walls in a single plane may not exceed 60 percent of the total façade length.
 - Regardless of façade length, all primary facades shall have at least one horizontal offset of the required percentage.

6.3.1 Form & Scale

Horizontal Articulation (All facades visible from public view)

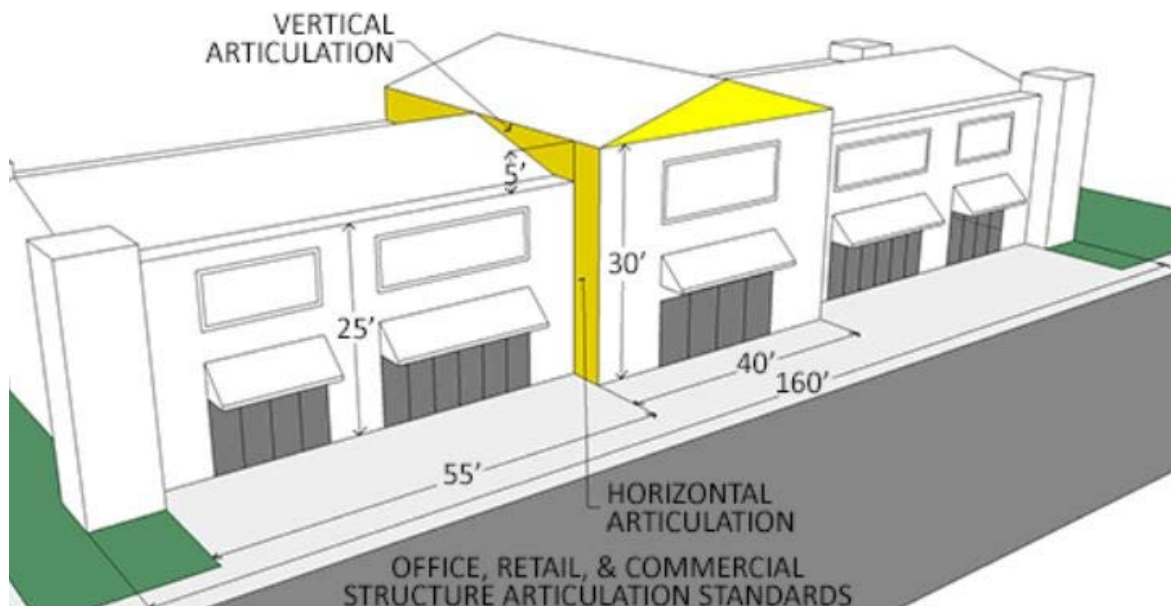
- Facades shall be broken up into vertical bays of 40-50 feet. Bays shall be articulated by pilasters, piers, and change in material texture, color, or variation in wall plane.
- Change in wall plane and material must occur every 50 to 80 feet.
- Buildings located on corners should employ a change in wall plane to address the corner.

- Vertical Articulation
 - A horizontal wall may not extend for a distance greater than three times its height without a change in elevation of at least 15 percent of such height.
 - The total length of all vertical elevation changes in the roofline shall be no less than 20 percent and no more than 40 percent of the total facade length.
 - Regardless of the façade length, all primary facades shall have at least one vertical elevation change.
 - Flat roofs with a parapet wall are permitted, provided the roofline meets the vertical articulation requirements.

6.3.2 Form & Scale

Vertical Articulation: (All facades visible from public view)

- Buildings shall have a clearly defined three part façade consisting of a base, middle, and top, which contribute to the human scale of the building.
- For single story buildings with exceptionally high ceiling plate, such as the case with large format retail, a string course shall be employed at a typical 2nd floor plate of 15-20 feet.
- Changes in height shall be employed at locations where wall plane is modulated.
- Buildings located on corners should employ a change in wall height to address the corner.



7.4.5 Building Entrance Standards

- Any front building entrance shall be set back from a drive aisle a minimum distance of 15 feet.
- Single-use or multitenant buildings over 50,000 square feet in size shall provide clearly defined, highly visible customer or employee entrances with the integration of awnings or similar architectural features.
- New or renovated commercial buildings shall have outdoor plazas, courtyards, or other pedestrian spaces at their main entrances.
 - Minimum size of pedestrian space shall be one (1) square foot of space per 100 square feet of building floor area or a minimum of 100 square feet of pedestrian space; whichever is greater.
 - All pedestrian spaces shall incorporate at least four (4) of the following:
 - Decorative landscape planters or wing walls that incorporate landscape
 - Pedestrian scale lighting, bollard, or other accent lighting
 - Special paving, such as colored/stained and sealed concrete, stamped concrete, brick or other unit paver.
 - Public art with a valuation of at least .05% of the total construction cost.
 - Seating such as benches, tables with attached seats, or low seating walls.
 - Architectural water structures, features, or fountains.
 - Other amenity approved by the Master Developer or Commercial Owner's Association (COA) Architectural Committee.

6.3.4 Entries

- *Entries should be clearly defined and highly visible.*
- *Entry articulation shall be the most prominent element of the façade.*
- *Entry features shall include a minimum of two (2) of the following: awnings, porches, porticos, arcades, colonnades, change in massing, change in wall plane, or change in roof forms.*
- *Multi-Story entry elements are permitted but must provide habitable space on upper floors.*

7.4.6 Canopy Standards

The following provisions apply to canopies associated with an ATM canopy, gas station canopy, drive-thru canopy, carport, and other similar auto oriented canopies.

- Canopies shall be constructed of roof building material consistent with that of the principal building.
- Canopies shall have pitched roofs, unless attached to the principal building utilizing a parapet roof type.
- Canopy columns shall be fully encased with primary masonry material that is complimentary to that used on the principal building, including brick and stone.

- The canopy band face shall be color consistent with the principal structure's exterior building materials and shall not be backlit or used as signage except that the business name may be displayed on the canopy band.
- Canopies shall be no higher than the principal building. In no case shall the canopy height exceed 20 feet.

7.4.7 Drive-Thru

Drive-thru facilities shall be located to the side or rear of the structure, unless site constraints limit such orientation, as determined by the Master Developer.

7.4.8 Overhead Doors

- Overhead doors shall not be located closer than 50 feet to a conforming residential lot.
- Overhead doors shall be oriented to the side or rear of the structure and not front or face a public right-of-way or public street with the following exceptions:
 - An automotive use with a maximum of four (4) single, service bays may orient toward a public street only when structural awnings of at least 3' 10" are provided over the extent of the overhead doors, or equivalent structural projections are provided in front of the overhead doors to reduce the visual impact of the service bays from the street.
 - A roll up, garage type door installed in a restaurant or bar may be permitted to face a public street if it is architecturally integrated into the building and provides a pedestrian connection with a covered outdoor patio area.
 - When physical site constraints prevent such orientation, the Master Developer may consider alternatives.



7.4.9 Loading Docks

- Loading docks shall not be located closer than 50 feet to a conforming residential lot.
- Loading docks shall be oriented to the side or rear of buildings, and oriented to not front the public right of way, not be visible or face a public street, main drive aisle, or patron parking lot.
- When physical site constraints prevent such orientation, the Master Developer may consider alternatives.

7.4.10 Service Court

- When multiple offices, commercial and industrial uses are planned, loading docks and delivery receivable areas shall be consolidated into common service courts located to the side or rear of the buildings.
- The access point into the service court shall be minimized in width in order to substantially screen the service court from a public street, main drive aisle or patron parking area, but allow for necessary vehicle maneuverability.
- Service courts shall be screened.

7.5 Roof Treatment

- Buildings can employ flat or sloping roofs.
- Decorative building parapets that provide distinctive silhouette should be considered for flat-roofed buildings; while projecting eaves, exposed beams and decorative brackets should be integrated into sloping roof designs.
- Sloping roofs should utilize concrete or clay tile in warm colors or standing seam metal roofs in a natural galvanized finish. Composition shingles may be allowed with the approval of the Master Developer or COA Architectural Committee.
- Roof overhangs and pitched roofs shall be incorporated into all building designs to achieve a roof form that is compatible with residential architecture. However, buildings large footprints located more than 200 feet from a single-family residential district, where applying a pitched roof would be impractical, may have a flat roof only if a parapet is used. Pitched roofs shall have a minimum pitch of 4:12. Long unarticulated roofs are not permitted.
- Parapets shall be used to conceal roof-mounted mechanical equipment on flat roofs on all sides.
- Where overhanging eaves are used, overhangs shall be no less than two (2) feet beyond the overhanging walls.
- Gable and hip roofs shall be symmetrically pitched between 4:12 and 8:12.
 - Shed roofs, porch roofs, and arcade roofs subordinate and attached to the primary structure, shall be pitched between 2:12 and 6:12.
 - Roof Type and Material Standards. Tile, metal or asphalt shingles (laminated 320 pound 30-year architectural grade asphalt shingles or better) shall be the



dominant roof material. Any other roofing material not stated shall not be permitted unless approved.

6.3.3 Roof Form

- *Gabled, hipped, or other sloped roof forms that relate to residential human scale, are encouraged.*

7.6 Façade Treatment

- Buildings will be designed to create a pedestrian scaled and varied streetscape, with the following characteristics.
 - Window openings composed and varied, with windows generally in a vertical proportion. Strip windows and curtain walls are not permitted.
 - Variation in building volume and plane to reduce the perceived scale of the building, and to create vertical interest along the street.
 - Architectural elements including projecting bay windows, balconies (in a non-repetitive pattern), canopies, pediments, moldings, etc., that further break up the mass of the building.
 - Four-sided and volumetric architecture, with materials that wrap around to the sides of the structure and that contribute to the three-dimensionality of the building.
 - Architectural delineation between the building's base, its upper levels and the roof silhouette.
- Corner and Terminus Elements: Buildings that occupy corner lots, or that terminate key views along the I-35 Frontage, Van Horn Trace, and Sub Collector Roads should introduce special architectural elements (e.g. towers, cupolas, gables, dormers, balconies, etc.) to reinforce the legibility of the Town Center, and to promote a varied and interesting streetscape.
- Primary Building Materials: A range of primary building materials may be used for buildings including Texas limestone or sandstone in light or warm tones, smooth finish brick in common, English or Flemish bond patterns, painted brick, smooth finished stucco (provided that Texas lime-stone or other stone is used as an accent material), light colored pre-cast concrete, or other similar or innovative materials deemed to be compatible and complementary.
- All portions of the building, visible from public streets and public parking must be treated in a similar manner. Materials should wrap around corners and define volumes rather than planes. Fiber-cement siding will be permitted only for the



exterior cladding of portions of the building not visible from public street, and not accessible by the public (e.g., rear or side facades, soffits, etc.).

- Corner and Corner Lots. Buildings on corner lots are to be finished with similar materials on all sides of the structure that contribute to the three-dimensionality of the building.
- Retail. Retail structures are to be finished with like materials on sides facing the main collector, sub collector or orchard parking but will not be less than two sides.
- Hotels. Hotels must be finished on all building faces facing the main and sub collector roadways, including adjacent faces, for a minimum of three finished faces.
- Office
 - Office buildings faces along the main collector and sub collector roads are to be finished with like materials as allowable by other sections of these guidelines, including street faces directly adjacent for a minimum of three sides. If orchard parking is provided for the office building, then that building face must also comply with like materials.
 - Office buildings lining the creek side frontage must be finished at a minimum of three sides on building faces that front the main or sub collector road ways. Buildings with two or more stories must be finished all four sides.
 - Pairs of office buildings with internal green spaces and courtyards must be finished with like materials on all building faces fronting the main collector or sub collector roads and adjacent building faces, including building faces fronting the internal courtyard.
 - Terminus office buildings that populate street ends at edges of the site must be finished on all faces fronting the main collector or sub collector roadways and adjacent faces, including faces fronting the property line.

6.3.1 Orientation

- *In some cases, such as corner lots, buildings will have more than one orientation and shall require more than one entry treatment.*

7.7 Parking and Screening

7.7.1 Surface Parking

Surface parking is to comply with design guidelines set forth by the PDD.

7.1 Design

- *Large parking areas over 100 spaces shall be divided into smaller “parking blocks” by establishing a clear pattern of internal streets spaced no less than 500 feet apart. Internal streets shall incorporate pedestrian and landscape amenities such as street trees and sidewalks.*
- *Curbed landscape islands shall be located at the end of each parking block and within each parking aisle at intervals of no greater than one island per 10 spaces. Each landscaped island shall be a minimum of 280 square feet.*
- *Where appropriate, use formal streetscape treatments such as parkway strips between street and sidewalk, regularly spaced canopy trees, and special paving to denote pedestrian zones.*
- *Pedestrian ways will be provided within large parking areas, and spaced no less than 500’ apart. Pedestrian ways will be wide enough for at least an 8’ pedestrian walk with supplemental landscaping for shaded pedestrian access from the parking lot to the surrounding buildings with limited interaction with auto traffic.*
- *When possible these internal parking medians will also assist in water quality and drainage by employing bio-swale and/or rain garden techniques.*

7.2 Access

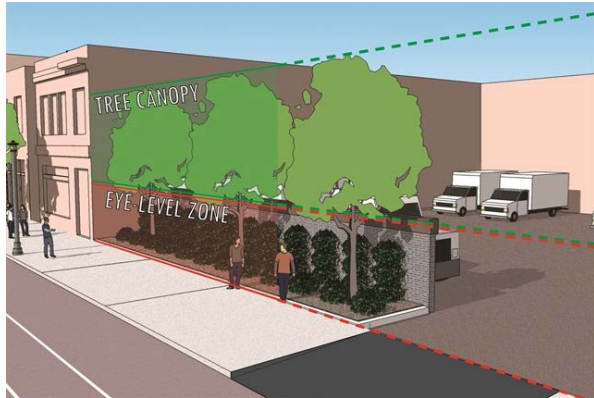
- *Mutual access shall be required between all parking areas.*
- *Lots with under 250 feet of frontage shall gain access from internal parking lot streets.*

7.7.2 Parking Garages

- Any exposed portion of the parking garage will be designed to appear as an integral extension of the adjacent buildings, with the same building materials and the same level of architectural detailing.
- A direct pedestrian linkage will be provided between parking facility and the I-35 Frontage, Van Horn Trace Frontage, and Sub Collector Roadway, through pedestrian passage and courtyard that is designed with landscaping and shading devices; this space will be well lit and include active ground level uses that make the passage safe and interesting throughout the day and evening.
- Vehicular access to the garage should be from the Internal Drives perpendicular to the Van Horn Trace Frontage and Sub Collector Roadways.
- Parking for bicycles and “car share” programs, should be provided in the garage, as applicable. Consideration should also be given to recharging stations for electrical vehicles, as feasible.



7.7.3 Screening



- Service Areas: All service areas (e.g. garbage and recycling rooms, mechanical areas, storage, utility and meter rooms, etc.) must be architecturally integrated within the body of the building, or architecturally screened from all public areas, and located to the maximum extent practicable on frontages other than the Highway Frontage, Van Horn Trace, and Sub Collector Roads
- Service and loading areas must be oriented away from public and pedestrian intensive areas along the I-35 Frontage, Van Horn Trace Frontage, and Sub Collector Roads.
- Service and loading areas along Interior Drives must be screened from predominant public view.

8.1 Service and Loading Area Screening

- Service and loading areas must be oriented away from public view from collector streets and/or primary parking areas.
- Services and loading areas must be concealed by screens at least as high as the equipment they hide and of color and material matching or compatible with the colors and material found on the façade of the primary building.

8.2 Mechanical Equipment Screening for Commercial Areas

The intent is to place equipment in less visible locations and obscure them from view while maintaining a safe environment.

- Rooftop mechanical equipment shall be hidden or screened with architecturally integral elements at least as high as the equipment to be screened. Parapet roof structures are approved for screening such equipment.
- Mechanical equipment at ground level shall be concealed by screens at least as high as the equipment they hide and of a color and material matching or compatible with the colors and material on the façade of the primary building. Screening may include architecturally integral wing walls and/or landscaping.
- Wall mounted equipment shall be screened with compatible materials and/or painted to match the structure.
- Mechanical equipment shall be located where their acoustics will not be disruptive to occupants or residents.
- All utilities shall be screened from streets and passageways.
- Screening of all electric utilities shall meet the minimum requirements of San Marcos Electric Utility or applicable electric provider's guidelines for granting safe access to equipment for operation and maintenance and for ventilation and cooling.

8.3 Parking Area Screening

- *Perimeters of parking areas adjacent to public right of way must be landscaped with shrubs reaching mature height of at least three feet and spaced a maximum of five feet on center or,*
- *trees planted 30 feet on center or,*
- *in combination with a decorative 3-foot-high fence with vines planted every 10' on center.*

8.4 I-35 Frontage Screening

- *Buildings and service and loading areas backing up to I-35 frontage must be screened with a minimum six foot tall masonry fence with pilasters at a spacing no greater than 12 feet and shrubs reaching a mature height of at least three feet and spaced a maximum of five feet on center and trees planted between 15 and 35 feet on center.*

8.6 Refuse and Recycling Dumpsters Screening for Commercial Areas

- *Screening shall be designed to reduce the escape of trash. Material and design shall be consistent with the primary building.*
- *All refuse and recycling dumpsters shall be screened from view from the street or public rights-of-way with landscaping or fencing at least as high as the dumpster*
- *Approved screening techniques include privacy fences, evergreen vegetative screens, landscape berms, existing vegetation or any combination thereof. In any case in which a fence/wall is constructed to provide screening, landscaping elements shall be incorporated along a majority of the fence/wall.*

7.8 Campus

- Buildings will be designed to reinforce the spatial definition of the perimeter streets. As such, buildings along all perimeter streets will be built within 10 feet of the property line to reinforce their spatial definition, and to promote ground level street-oriented activity. Front yard landscaping, in conjunction with sidewalk extensions and building entries, should be combined to create an interesting and varied streetscape. At least 75% of the frontage along each block face should be lined with buildings, with their primary or secondary public entries oriented to the fronting sidewalk.
- Buildings within the campus should have a clear architectural relationship with one another, employing common building materials or architectonic elements, while creating visual diversity and interest.
- Building entries should have strong architectural expression to promote a sense of orientation for visitors to the complex. Entries should be situated along key pedestrian routes and adjacent to public open spaces.

- Building activities throughout the campus should be oriented to perimeter streets and to interior pedestrian ways and opens spaces to the maximum extent practicable.
- Signage Program. A coordinated signage program for each subarea within the campus must be prepared and submitted for review and approval. The sign program must conform to City of San Marcos standards, as well as the following guidelines.
 - A clear hierarchy of signage should be established within each of campus subareas, ranging from larger monument or “tower” signage oriented to smaller monuments, to signs that are in scale and proportion with the buildings on which they are mounted.
 - The signage program for the mixed-use retail center should be visually subservient to the landscape, public art and other identity treatments that establish Trace along the I-35 frontage.
 - The one exception are major architectural signage element in conjunction with the I-35 identity treatment designed as a high quality landmark feature provided by the Master Developer.
 - Advertising, electronic, or flashing signs are prohibited.

11.2 Commercial Signage (C):

The symbols indicated as “C” on the Signage Plan represents signage for the Commercial Parcels of Trace. These monuments will serve to announce major single occupants or multiple tenant spaces in the areas designated on the land plan.

- *The square footage (SF) of the signage will be calculated as the smallest rectangle around the lettering comprising the message (logo SF will be calculated separately).*
- *The height will be calculated using the highest elevation of the lettering above grade. The logo may be higher.*
- *The structure supporting the sign will be no taller than 20’ along I-35.*
- *Signage along I-35 will be complimentary and sub-servient to the Major Entry Feature designated at I-35 and be limited to 260 sf (each side if two sided).*
- *Signage along other roads (Posey and Old Bastrop) will be limited to 120 sf (each side if two sided) and 12’ height.*
- *The height will be measured from the grade on which the sign stands or the adjacent roadway elevation whichever is higher.*
- *Potential materials shall be limited to: (excluding temporary marketing signs):*
 - *Stone Masonry*
 - *Brick Masonry*
 - *Metal (Aluminum, Corten, Raw Steel, or Stainless Steel) for letter and possible sign background.*
 - *Wood (limited use, treated, stained or painted and protected from decay)*
 - *Stucco*
 - *Concrete (Board Formed, Stained)*

11.6 Directional Signage (D)

- *The symbol indicated as “D” on the Signage Plan represents directional signage to be placed throughout Trace. These monuments will serve to announce various elements of Trace. These signs will be concise, directional and informative to offer assistance to visitors to orient themselves to the major elements of Trace.*
- *The height of the lettering will be no greater than 6’ above grade/or adjacent road elevation.*
- *The SF of the entire composition of sign and structure will be limited to 48 sf.*
- *The structure height will be limited to 8’.*
- *The height will be measured from the grade on which the sign stands or the adjacent roadway elevation whichever is higher.*
- *Potential materials shall be limited to:*
- *Stone Masonry*
- *Brick Masonry*
- *Metal (Aluminum, Corten, Raw Steel, or Stainless Steel) for letter and possible sign background.*
- *Wood (limited use, treated, stained or painted and protected from decay)*
- *Stucco*
- *Concrete (Board Formed, Stained)*
- *Izone or similar signage material for print*

7.9 Grocery Store

A grocery store is an important element of the Town Center to ensure its role as a convenient neighborhood retail destination for Trace and area residents. A facility permitted within the Town Center, provided that it meets the following conditions:

- The design of the grocery store and its exterior building materials are to be consistent with the architectural guidelines set forth in previous sections of this document and is determined by the Master Developer to be in keeping with the intended identity and character of the Town Center.
- The design and siting of the grocery store does not devalue or denigrate adjacent properties.



- Truck docks, service areas and the blank side and rear walls of the grocery store are visually and acoustically screened with “liner uses” (e.g. retail, shop house lofts), architectural noise walls of up to 12 feet in height, and/or landscaping.
- The siting and orientation of the grocery store contributes to the activation of the Town Center, with entries on the street and/or convenient pedestrian linkages from the front of the store to the street.
- The parking for the grocery store is provided in the shared orchard parking lot, or in acceptably designed structured parking beneath or adjacent to the store. The design of the orchard parking must comply with the landscaping guidelines set forth in the PDD.

7.10 Gas Stations & Convenience Stores

A gas station is a facility where gasoline or other fuels are sold and where maintenance and minor automobile repair services may be conducted. A gas station consists of a gas bar with gasoline outlets and typically other associated facilities such as car washes, automotive services. A gas station and/or convenience store is permitted provided it meets the following conditions:

- Respond to the positive elements of the context through such means as building height, setbacks, building orientation and architectural styles
- Provide ample landscaping, in combination with building orientation, to enhance the streetscape and define the street edge when setting building structures back from the street is unavoidable
- Design all sides of buildings and pump islands with a consistent architectural style to enhance the streetscape.
- Locate vehicular access points to the site as far away as possible from street intersections.
- Use trees, shrubs and low walls to screen cars from view while allowing eye level visibility into the site.
- Provide significant architectural or landscape features at the corner on corner sites in order to emphasize the public streets and enhance the streetscape
- Design buildings to include defined spaces to accommodate signs that respect building scale, architectural features, signage uniformity and established streetscape design objectives.



- Locate and design ground-mounted and wall-mounted signs to complement the character and scale of the area and promote an active, pedestrian friendly environment. Integrate landscape features with ground-mounted signs
- Restrict temporary and portable signs. Prohibit billboards, revolving signs and roof signs
- Design sign illumination to be task-oriented and avoid glare/light spillover toward adjacent land uses.
- Storefronts and windows will have sill heights starting @ 36" minimum.

7.11 Building Fixtures

- Building Lighting is important in adding to the attractiveness of the Trace Town Center, creating a sense of safety and security. Light equipment selections and lighting design should be made with a goal of eliminating glare, light trespass and light pollution.
- Building Lighting layout will be coordinated with proposed landscaping, existing trees, and landscape lighting fixtures to maximize light distribution and avoid conflicts.
- Light fixtures will be designed and lamped for both pedestrian and vehicular purposes.
- Only along Van Horn Trace and Sub Collector Roadways, 30-foot "Cobrahead" fixture be used. Where it is used, ornamental pole bases, poles and mast arms are encouraged.
- Fixtures will be adapted to comply with light trespass considerations as required.
- Light Fixtures must meet applicable City of San Marcos standards, and be designed to avoid light pollution by employing cut-offs that prevent light from being emitted above the horizontal plane.

6.3.7 Lighting for Commercial Areas

The intent is to enhance day and night time appearances, establish a safe environment for occupants and employees and minimize light pollution, while minimizing glare and light trespass onto adjacent properties. The lighting of all parking areas, pedestrian walkways, entrances, and exterior portions of the site shall be designed for its specific task and shall comply with the provisions of the existing City Code of Ordinances, Subpart B – Land Development Code, Chapter 6, Article 5 attached in the Appendix.

- *Lighting shall be: vandal-proof; compatible with building architecture; and, scaled (dimension and intensity) to complement its location context.*
- *Lighting shall not be obscured by landscaping.*

7.12 Signage and Storefront Guidelines

- These guidelines are designed to help ensure the erection of quality signs that communicate their message in a clear fashion. The Master Developer may interpret these design guidelines with some flexibility when applied to specific signs or projects, as not all design criteria may be practical for each site or location. In some circumstances, one guideline may be modified to facilitate compliance with another guideline determined by the Master Developer to be more important in the particular case. The overall objective is to ensure that the intent and spirit of these guidelines are followed when developing criteria for TRACE.

7.12.1 Wall Signs

- **Description.** Wall Signs are permanent signs constructed of a durable material such as metal, stone, acrylic, or other engineered product. Wall Signs are structurally attached to a building along a plane that is parallel to the building face to which the sign is attached. Signs on windows, attached to windows, or viewed through windows are covered in section 7.12.2.
- **Street Type and Frontage.** Permitted on all street types.
- **Sign Dimensions - Area.** Maximum 15% total wall area per wall. (FIG. 1)
- **Sign Dimensions - Width.** Maximum 75% of associated tenant space width (FIG.1)

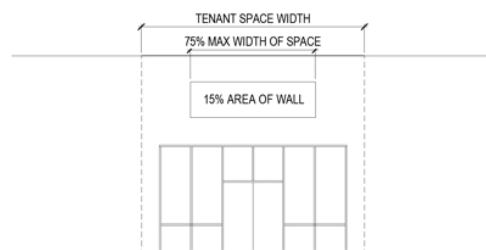


FIG.1

- **Illumination.** Lighting may be: internal and external.
- **Electronic Message Centers/Message Board.** EMCs and Message Boards are not allowed on a building face.
- **Special Conditions**
 - Wall signs shall be constructed of a rigid, durable material such as metal or acrylic and shall be structurally mounted to face of a building.
 - Electrical componentry shall be concealed behind the sign or cabinet.
 - Multiple wall signs shall be permitted, provided that the cumulative area of all signs does not exceed the maximum permitted area and that the number of distinct attached sign types does not exceed three (3). (FIG.2)
 - No wall sign, regardless of overall size, shall cover an area that exceeds 75% of the width of a single wall plane. (FIG.1)

- All wall sign installations shall preserve a clear area between the extreme edge of the sign and any terminating building corner, or between the extreme edge of the sign and the boundary of any demised space. The total clear area shall be equal to a minimum of 25% of the width of the wall or the demised space, whichever is less. (FIG.3)
- The distance between the inner edge of a wall sign and the outer face of the wall to which it is attached shall not exceed 18 inches.

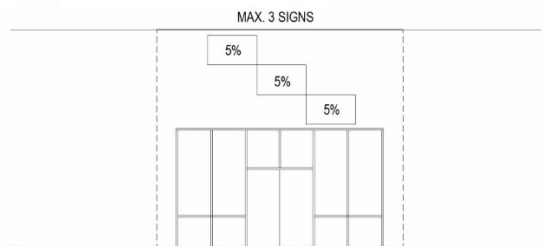


FIG.2

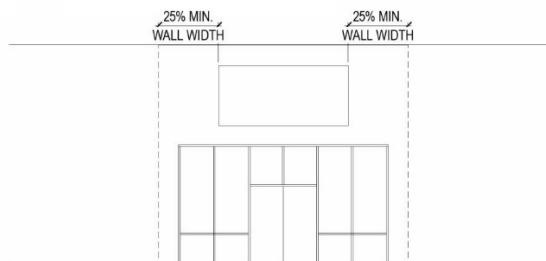


FIG.3

7.12.2 Window Signs

- **Description.** A window sign is generally affixed to a window for the purpose of being viewed from the exterior of a building.
- **Street Type and Frontage.** Permitted on all street types except local streets.

- **Sign Dimensions - Area.** Maximum 25% of total window area. (FIG.4)

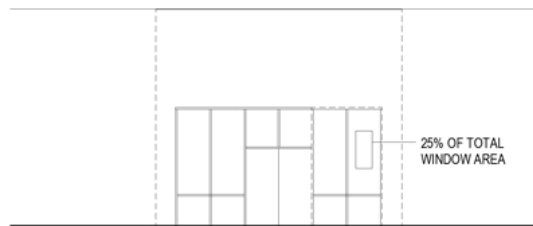


FIG.4

- **Number of Signs.** Only one window sign per tenant, per elevation allowed.
- **Illumination.** Not allowed.
- **Additional standards**
 - Window signs shall not obscure the visibility into a tenant space. Every effort shall be made to integrate window signs with store window displays.
 - Flashing, scrolling, or moving signs that are readily visible from the public right of way are not permitted.
 - No fluorescent colors or reflective material shall be used for window signs.
 - Window signs are only permitted on the first story of the building.
 - Mannequins and storefront displays of merchandise sold are not considered window signs.
 - Window signage that depict hours of operation and open/closed are permitted 1 per storefront.

7.12.4 Canopy Signs

- **Description.** Canopy signs may be used on freestanding canopies or canopies attached to buildings. Signs on the fascia will meet the same standards as Canopy Signs.
- **Street Type and Frontage.** Permitted on all street types.
- **Sign Dimensions - Area.** Maximum 35 sf. (FIG.5)
- **Sign Dimensions - Height.** Maximum 3 ft. (FIG.5)
- **Sign Dimensions - Width.** Canopy on drive-through canopies shall be limited to 75 percent of the canopy width. (FIG.6)
- **Number of Signs.** Signs per tenant building face, max. 1
- **Illumination.** External illumination is not allowed
- **Electronic Message Centers/Message Boards.** EMCs and Message Boards are not allowed
- **Additional Standards**
 - Vertical clearance of nine feet above grade from the bottom of the canopy is required.

- The area of canopy signs shall be counted towards the maximum wall sign area allowed for that elevation. Individual letters or graphics may be internally illuminated, glow with a halo-illumination effect, or glow through the front faces.
- Flashing lights, or exposed raceways, conduits, or transformers are prohibited.
- Shall not extend above the second floor ceiling or the top of the roof, whichever is less.
- Shall consist only of letters and/or geometric designs and not constructed as part of a sign can or cabinet.

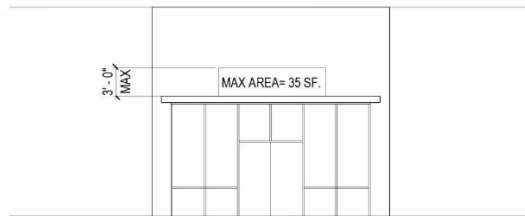


FIG.5

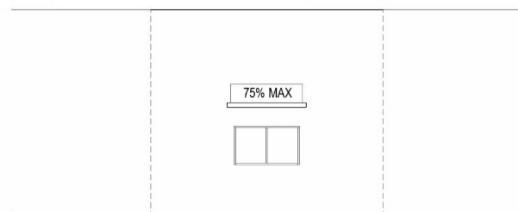


FIG.6

7.12.5 Prohibited Signs

- Flat signs, including flat plastic/acrylic signs; all signs shall have significant depth and relief.
- Roof Signs (erected upon any part of a roof or parapet of a building).
- Signs that flash, blink, scroll, or move or simulate movement.
- No Placards, flags, or pennants of any type are allowed.
- No exposed raceways, crossovers, conduits, neon tube connectors or transformers are allowed.
- Neon tubing framing windows is not allowed.

