

## PROPERTY FOR SALE

8 RAILROAD AVE N, HAINES CITY, FL
877.518.5263 | SRDcommercial.com | 114 N. Tennessee Ave. Lakeland, FL 33801

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## EXECUTIVE SUMMARY



## HAINES CITY CITRUS GROWERS INDUSTRIAL PROPERTY

The subject property is located on the southside of Lake Tracy in Downtown Haines City, FL. According to the Polk County Property Appraiser, there is $175,564+/-$ SF on 13.28 +/- acres of land. Located in Central Florida, there are more than 20,000,000 people within a 5 hour drive-time of the subject, excellent for distribution businesses.

Built in 1925, this property has served as a citrus packing facility, and the next use will likely be industrial, but is up to the imagination of a buyer.

| Site Address: | 8 Railroad Ave. N., Haines City, FL 33844 |
| :--- | :--- |
| County: | Polk |
| PIN (Property Identification Numbers): | 272729790800000080,272729787000001001, <br> 272729786500000110 |
| Land Size: | $13.28+/-$ acres |
| Building Size (Polk PA): | $175,564+/-$ SF |
| Year Built: | 1925 |
| Property Use: | Citrus Packing Plant |
| Utilities: | See Page 15 |
| Ceiling Height: | Up to 40' |
| Zoning: | Industrial Heavy (Haines City) |
| Taxes: | $\$ 49,048.20$ (2018) |
| Traffic Count: | 26,500 cars/day on US Hwy 92 |
| Asking Price: | $\$ 3,950,000$ |



## REGIONAL LOCATION MAP

Located in the Lakeland - Winter Haven MSA, between Tampa and Orlando, in the center of the I-4 Corridor.



DEMOGRAPHICS MAP
1, 3, 5 mile radius
15,45 minute drive time

BENCHMARKDEMOGRAPHICS

|  | 1 Mile | 3 Miles | 5 Miles | 15 Mins | 45 Mins | Polk | FL | US |
| :---: | :---: | :---: | :---: | :---: | :---: | :---: | :---: | :---: |
| Population | 6,533 | 28,874 | 52,853 | 90,233 | 1,243,480 | 667,696 | 20,875,686 | 330,088,686 |
| Households | 2,042 | 10,068 | 19,657 | 34,065 | 447,166 | 249,123 | 8,152,541 | 124,110,001 |
| Families | 1,490 | 7,289 | 14,217 | 23,979 | 315,584 | 173,193 | 5,273,287 | 81,631,156 |
| Average Household Size | 3.15 | 2.85 | 2.67 | 2.63 | 2.73 | 2.63 | 2.51 | 2.59 |
| Owner Occupied Housing Units | 1,135 | 6,766 | 14,598 | 24,357 | 286,948 | 169,591 | 5,193,134 | 78,262,285 |
| Renter Occupied Housing Units | 906 | 3,302 | 5,058 | 9,708 | 160,218 | 79,532 | 2,959,407 | 45,847,716 |
| Median Age | 31.9 | 38.4 | 44.6 | 44.3 | 38.4 | 41.4 | 42.3 | 38.3 |
| Income |  |  |  |  |  |  |  |  |
| Median Household Income | \$37,561 | \$37,432 | \$41,358 | \$42,037 | \$51,093 | \$47,429 | \$52,098 | \$58,100 |
| Average Household Income | \$49,226 | \$50,439 | \$55,831 | \$57,388 | \$70,937 | \$64,107 | \$75,281 | \$83,694 |
| Per Capita Income | \$16,039 | \$17,757 | \$20,912 | \$22,012 | \$26,051 | \$24,475 | \$29,913 | \$31,950 |

Trends: 2018-2023 Annual Growth Rate

| Population | $1.82 \%$ | $2.16 \%$ | $2.30 \%$ | $2.16 \%$ | $2.19 \%$ | $1.55 \%$ | $1.41 \%$ | $0.83 \%$ |
| :--- | :--- | :--- | :--- | :--- | :--- | :--- | :--- | :--- |
| Households | $1.69 \%$ | $2.02 \%$ | $2.19 \%$ | $2.06 \%$ | $2.07 \%$ | $1.48 \%$ | $1.36 \%$ | $0.79 \%$ |
| Families | $1.62 \%$ | $1.96 \%$ | $2.14 \%$ | $2.02 \%$ | $2.02 \%$ | $1.42 \%$ | $1.30 \%$ | $0.71 \%$ |
| Owner HHs | $2.66 \%$ | $2.72 \%$ | $2.75 \%$ | $2.71 \%$ | $2.62 \%$ | $2.07 \%$ | $1.91 \%$ | $1.16 \%$ |
| Median Household Income | $2.34 \%$ | $2.69 \%$ | $3.28 \%$ | $3.26 \%$ | $2.46 \%$ | $2.65 \%$ | $2.52 \%$ | $2.50 \%$ |

ood population density with almost 30,000 people in 3 miles, and more than 1.2 million in a 45 minute drive-time.

## BENCHMARK DEMOGRAPHICS

1 Mile 3 Miles 5 Miles 15 Mins 45 Mins Polk FL US
Households by Income

| $<15,000$ | $16.80 \%$ | $16.10 \%$ | $14.70 \%$ | $14.60 \%$ | $10.70 \%$ | $12.10 \%$ | $11.70 \%$ | $11.20 \%$ |
| :--- | ---: | ---: | ---: | ---: | ---: | ---: | ---: | ---: |
| $\$ 15,000-\$ 24,999$ | $13.90 \%$ | $14.30 \%$ | $12.60 \%$ | $12.10 \%$ | $10.90 \%$ | $11.70 \%$ | $10.60 \%$ | $9.40 \%$ |
| $\$ 25,000-\$ 34,999$ | $15.00 \%$ | $15.60 \%$ | $13.70 \%$ | $13.60 \%$ | $11.50 \%$ | $11.80 \%$ | $10.70 \%$ | $9.30 \%$ |
| $\$ 35,000-\$ 49,999$ | $18.30 \%$ | $18.10 \%$ | $17.60 \%$ | $17.30 \%$ | $15.60 \%$ | $16.60 \%$ | $14.70 \%$ | $12.80 \%$ |
| $\$ 50,000-\$ 74,999$ | $19.50 \%$ | $18.10 \%$ | $19.40 \%$ | $19.10 \%$ | $19.60 \%$ | $19.90 \%$ | $18.70 \%$ | $17.60 \%$ |
| $\$ 75,000-\$ 99,999$ | $7.80 \%$ | $7.90 \%$ | $10.50 \%$ | $11.00 \%$ | $12.30 \%$ | $12.30 \%$ | $11.90 \%$ | $12.50 \%$ |
| $\$ 100,000-\$ 149,999$ | $5.80 \%$ | $6.70 \%$ | $7.70 \%$ | $8.00 \%$ | $11.40 \%$ | $9.90 \%$ | $12.10 \%$ | $14.40 \%$ |
| $\$ 150,000-\$ 199,999$ | $1.00 \%$ | $1.60 \%$ | $2.10 \%$ | $2.10 \%$ | $3.70 \%$ | $2.90 \%$ | $4.50 \%$ | $6.00 \%$ |
| $\$ 200,000+$ | $1.80 \%$ | $1.60 \%$ | $1.90 \%$ | $2.20 \%$ | $4.30 \%$ | $2.90 \%$ | $5.10 \%$ | $6.70 \%$ |

Population by Age

| $\mathbf{0 - 4}$ | $9.10 \%$ | $7.40 \%$ | $6.30 \%$ | $6.10 \%$ | $6.10 \%$ | $6.00 \%$ | $5.30 \%$ | $6.00 \%$ |
| :--- | ---: | ---: | ---: | ---: | ---: | ---: | ---: | ---: |
| $\mathbf{5 - 9}$ | $8.60 \%$ | $7.20 \%$ | $6.10 \%$ | $6.00 \%$ | $6.10 \%$ | $6.00 \%$ | $5.40 \%$ | $6.20 \%$ |
| $\mathbf{1 0 - 1 4}$ | $7.80 \%$ | $6.80 \%$ | $5.90 \%$ | $5.90 \%$ | $6.20 \%$ | $6.00 \%$ | $5.60 \%$ | $6.30 \%$ |
| $\mathbf{1 5 - 1 9}$ | $7.00 \%$ | $6.00 \%$ | $5.30 \%$ | $5.40 \%$ | $6.20 \%$ | $5.90 \%$ | $5.70 \%$ | $6.40 \%$ |
| $\mathbf{2 0 - 2 4}$ | $7.20 \%$ | $5.80 \%$ | $5.20 \%$ | $5.20 \%$ | $6.70 \%$ | $5.90 \%$ | $6.30 \%$ | $6.90 \%$ |
| $\mathbf{2 5 - 3 4}$ | $15.00 \%$ | $12.70 \%$ | $11.40 \%$ | $11.60 \%$ | $14.30 \%$ | $12.70 \%$ | $13.20 \%$ | $13.90 \%$ |
| $\mathbf{3 5 - 4 4}$ | $12.30 \%$ | $11.20 \%$ | $10.20 \%$ | $10.50 \%$ | $12.40 \%$ | $11.40 \%$ | $11.70 \%$ | $12.50 \%$ |
| $\mathbf{4 5 - 5 4}$ | $10.10 \%$ | $9.90 \%$ | $9.90 \%$ | $10.50 \%$ | $12.50 \%$ | $11.80 \%$ | $12.70 \%$ | $12.80 \%$ |
| $\mathbf{5 5 - 6 4}$ | $9.90 \%$ | $11.30 \%$ | $12.20 \%$ | $12.80 \%$ | $12.40 \%$ | $13.10 \%$ | $13.60 \%$ | $13.00 \%$ |
| $\mathbf{6 5 - 7 4}$ | $7.40 \%$ | $11.60 \%$ | $14.60 \%$ | $13.80 \%$ | $10.10 \%$ | $12.10 \%$ | $11.50 \%$ | $9.40 \%$ |
| $\mathbf{7 5 - 8 4}$ | $4.10 \%$ | $7.50 \%$ | $9.60 \%$ | $8.90 \%$ | $5.10 \%$ | $6.60 \%$ | $6.30 \%$ | $4.60 \%$ |
| $\mathbf{8 5 -}$ | $1.70 \%$ | $2.70 \%$ | $3.30 \%$ | $3.30 \%$ | $1.90 \%$ | $2.40 \%$ | $2.70 \%$ | $2.00 \%$ |

Race and Ethnicity

| White Alone | $46.70 \%$ | $55.70 \%$ | $64.10 \%$ | $64.50 \%$ | $68.80 \%$ | $72.30 \%$ | $73.00 \%$ | $69.90 \%$ |
| :--- | ---: | ---: | ---: | ---: | ---: | ---: | ---: | ---: |
| Black Alone | $28.60 \%$ | $23.30 \%$ | $18.00 \%$ | $20.60 \%$ | $14.50 \%$ | $15.30 \%$ | $16.40 \%$ | $12.90 \%$ |
| American Indian Alone | $0.50 \%$ | $0.50 \%$ | $0.40 \%$ | $0.40 \%$ | $0.50 \%$ | $0.50 \%$ | $0.40 \%$ | $1.00 \%$ |
| Asian Alone | $0.80 \%$ | $1.10 \%$ | $1.20 \%$ | $1.40 \%$ | $3.90 \%$ | $1.90 \%$ | $2.80 \%$ | $5.70 \%$ |
| Pacific Islander Alone | $0.00 \%$ | $0.00 \%$ | $0.00 \%$ | $0.10 \%$ | $0.10 \%$ | $0.10 \%$ | $0.10 \%$ | $0.20 \%$ |
| Some Other Race Alone | $20.60 \%$ | $16.70 \%$ | $13.50 \%$ | $10.30 \%$ | $8.60 \%$ | $7.00 \%$ | $4.30 \%$ | $6.90 \%$ |
| Two or More Races | $2.80 \%$ | $2.80 \%$ | $2.70 \%$ | $2.70 \%$ | $3.70 \%$ | $3.00 \%$ | $3.00 \%$ | $3.40 \%$ |
| Hispanic Origin (Any Race) | $49.80 \%$ | $42.30 \%$ | $35.90 \%$ | $28.90 \%$ | $32.70 \%$ | $22.60 \%$ | $25.90 \%$ | $18.30 \%$ |



Located in close proximity to US 27. The
MARKET AREA MAP trade area has a good mixture of Commercial, Industrial, and Residential uses.


Located within an active trade area with good traffic
TRADE AREA MAP counts. US 27 is a major corridor for truck traffic.


Located in close proximity to Downtown Haines City.

NEIGHBORHOOD AERIAL


The subject property features $980+/-$ FT of


## Electrical Service

- Duke Energy
- 12.47 kV distribution feeding the facility
- Three-phase service from several locations:
- 1500 KVA Underground 277/480 Volt transformer.
- Two 300 KVA Overhead 277/480 Volt banks.
- 300 KVA Underground 277/480 Volt transformer on primary Underground loop
- 500 KVA Underground 277/380 Volt transfomer on primary Underground loop
. Feeder K20-5MW current capacity.


## Water

- City of Haines City
- Ten inch water line in the right of way in front of the property
- Currently three meters feed the property (one 3/4" and two 2" meters).
- In addition, there are private fire hydrants on the property capable of 1500 GPM each.


## Sewage

- City of Haines City
- Eight inch gravity sewer line capable of 500 GPM


## Natural Gas

- Florida Public Utilities
- Served by 2 " line with 50 PSI





The newest building on the premises is improved as cooler space, and is leased. The gross annual rent is $\$ 504,000$.

The lease is short term and would need to be renewed in order to contribute much value beyond the physical value of the building.

## Cold Storage

The backside of the building that fronts Lake Tracy has a truck court and dock high loading.

This area of the property could potentially be redeveloped for residential along the lakefront, or other uses, but the highest and best use probably involves keeping this building in production, without altering current operations.



## Industrial Heavy (IH)

These districts are intended primarily for heavy manufacturing and closely related uses. It is intended to preserve such lands for the functions of heavy industrial activity, wholesaling, warehousing, and distribution. To allow maximum latitude for operations, performance standards are applied at district boundary lines. It is not intended to permit uses not of the general character indicated as principal uses, nor to permit general commercial operations, residential or institutional uses within the district.

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VIH
    CG
    R-3X
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```CBDX
```

```CBD
```


# (11)SVN RALSTON DANTZLER 

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