

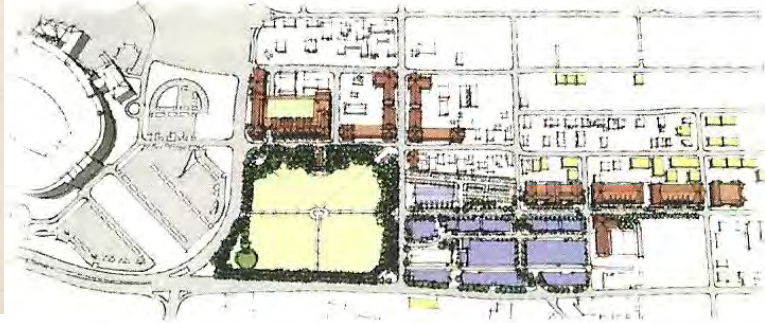
Daytona Beach Midtown Master Plan Development Standards & Design Guidelines



Huffman Associates / Studio for Architecture Urban Design & Planning Projects



South Campus Master Plan Florida State University



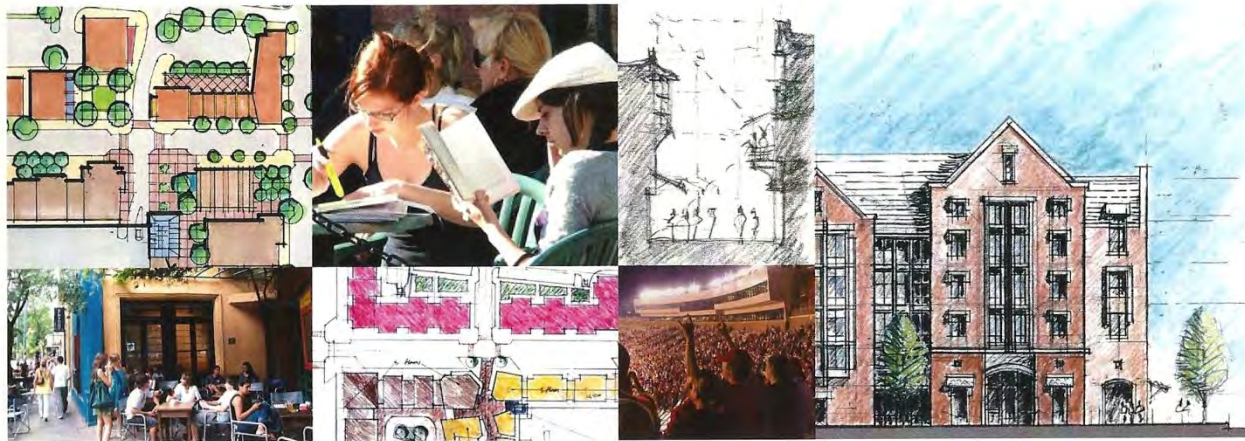
Florida A & M School of Architecture Funded Research & Planning

Small Town Planning Studies for: Dunnellon, Inglis, Newberry, Panacea, Port St. Joe, Florida
Florida Department of Community Affairs

Downtown Nassau Redevelopment Project
Office of Museum & Antiquities - \$20,000

Downtown Jacksonville Corridor Study
City of Jacksonville - \$160,000

Seminole Boosters Phase 1 Conceptual Master Plan



Florida State University • Tallahassee, Florida

Midtown Masterplan

City of Daytona Beach + Florida A & M University SCHOOL OF ARCHITECTURE

Midtown Master Plan, Daytona Beach, FL

Florida A & M University, School of Architecture

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1. Plan Development Process

Plan Development Process:

1.1 Introduction

The City of Daytona Beach, Florida and the Midtown CRA contracted with the Florida A & M University School of Architecture to develop a Master Plan with Design and Development Standards that will guide the growth and land development process for the Midtown CRA area. The plan is based on historical research, existing constraints and opportunities and a series of workshops with citizens, home and business owners and neighborhood participants. The plan has also been developed with input and assistance from the Midtown Board and the Daytona Beach Redevelopment Department.

This document is a record of the history and evolution of the Midtown District in Daytona Beach, Florida. It is also a blueprint for positive growth and change over the next 50 years. It is based on participatory community planning plus new urbanist design and place making principles. The Master Plan and Development Standards chart a course for preserving the historic buildings, places and significant institutions in Midtown; while creating incentives and quality standards for new growth.

The Master Plan and Development Code forms the foundation of a *place based* economic development strategy.

“Place based planning with design standards and guidelines are a catalyst for economic development”

Hazel Borys, Placemaking Consultant
Common Ground – Real Estate & Smart Growth

Midtown Masterplan

City of Daytona Beach + Florida A & M University SCHOOL OF ARCHITECTURE



The architectural landmarks and unique district character includes an eclectic mix of styles, materials, and color. These existing architectural patterns will inform the new design guidelines for the district.



Plan Development Process :

1.2 Overview of the Midtown District Area

The Midtown CRA is an historic area that is located in the geographic center of Daytona Beach approximately half way between the Atlantic coast beaches on the east and the International Speedway and Airport area / I - 95 on the west.

Midtown has many assets, including:

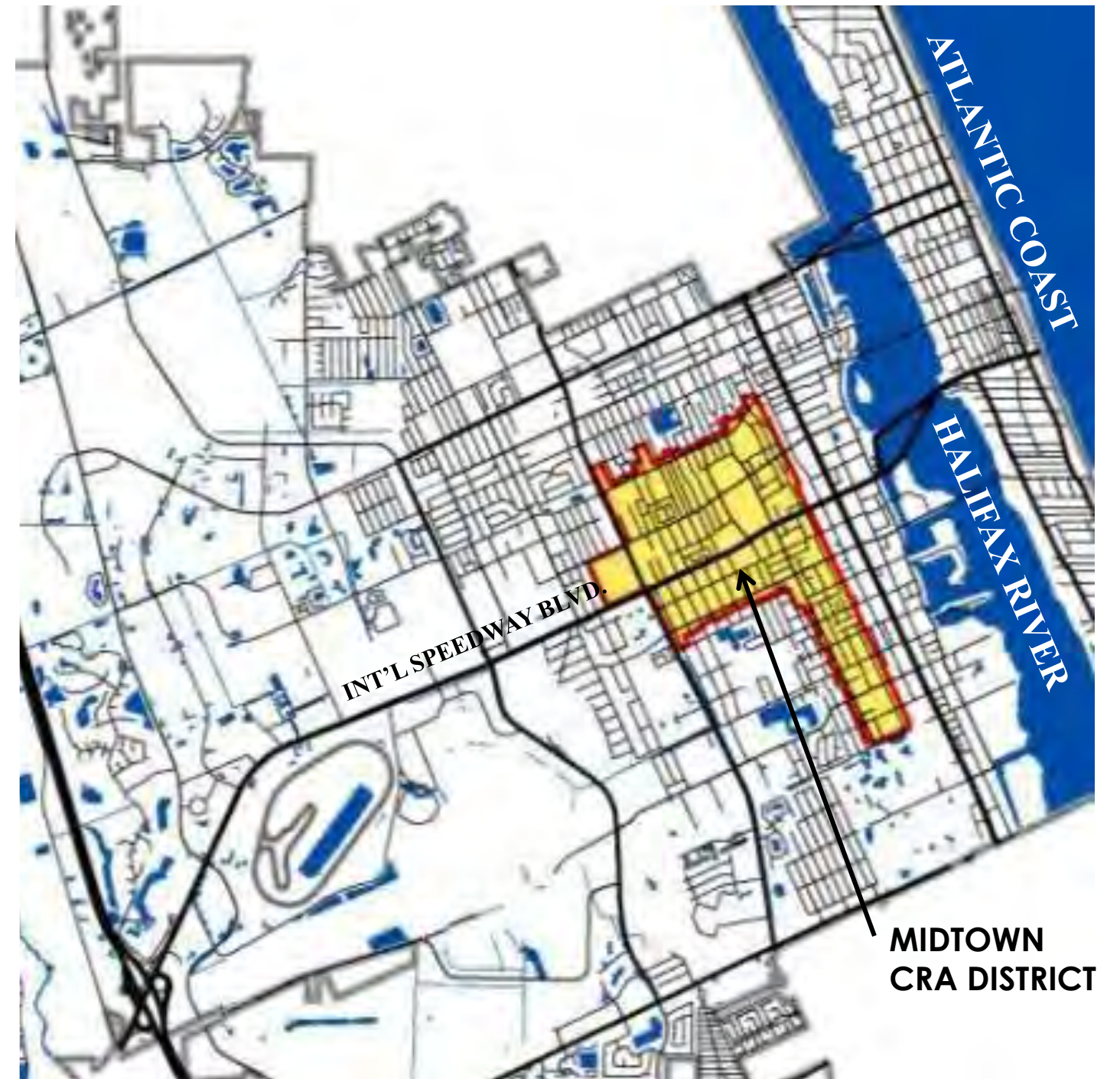
- an excellent location
- a rich history of African American culture
- Bethune Cookman University
- a significant area of contributing historic structures
- an interconnected street grid with good connections to the larger community
- ISB has the potential to be an attractive “urban boulevard” with new mixed use development.
- affordable vacant property that can accommodate growth and redevelopment.

There are also many challenges:

- there is a deficit of business and home ownership
- a decreased tax base in relation to CRA area
- income levels are generally lower than the adjacent community
- a concentration of public housing
- outdated infrastructure & inadequate stormwater facilities
- inadequate parks, playgrounds, recreation and public spaces
- significant areas of vacant property
- a lack of basic services and shopping
- ISB is a “cut through highway” that divides north & south Midtown.
- no identifiable Midtown “center”

The Midtown Master Plan presents an urban design and community planning framework, including *Development Standards* that are specifically designed for Midtown, that will provide a guiding vision for future growth. The plan acknowledges and preserves the assets listed above while addressing the challenges and barriers that are limiting reinvestment in the area.

Midtown CRA District: Location & Context



Plan Development Process : 1.3 Community Workshops

An essential element of the planning process was the community's involvement. There were 6 workshop – neighborhood review discussions as the plan was developed. Workshop locations included the Dickerson Community Center, Bethune Cookman University and City Hall. The Midtown CRA Board members provided leadership and guidance throughout the process.

Workshop 1 Summary

- There was a high turn-out; approximately 65 people.
- Attendees included a diverse group of Midtown citizens and area residents, and representatives from the City of Daytona Beach, Volusia County, Daytona State College, and churches.
- The community wants attractive streets with trees, businesses, nice street lights, underground utilities, a family-friendly, livable community.
- Orange Avenue is used by Daytona Beach citizens as a short-cut between Nova and Beach St. to access Atlantic Blvd.
- Crossing ISB is dangerous and difficult for children who want to go to the local softball fields and the large recreation center.
- The health, well-being, and safety of the citizens is essential.
- There is a strong sense of community and desire to better Midtown amongst the key stakeholders, however, the groups have not formed a cohesive coalition.
- The community is open to national restaurant and retail uses at ISB.
- The Academic Institutions want a safe, attractive environment for their students, and space for future expansions.
- Flooding is a problem in certain areas.
- Safety and livability need improvement.



Workshop 1 - Community Planning Session

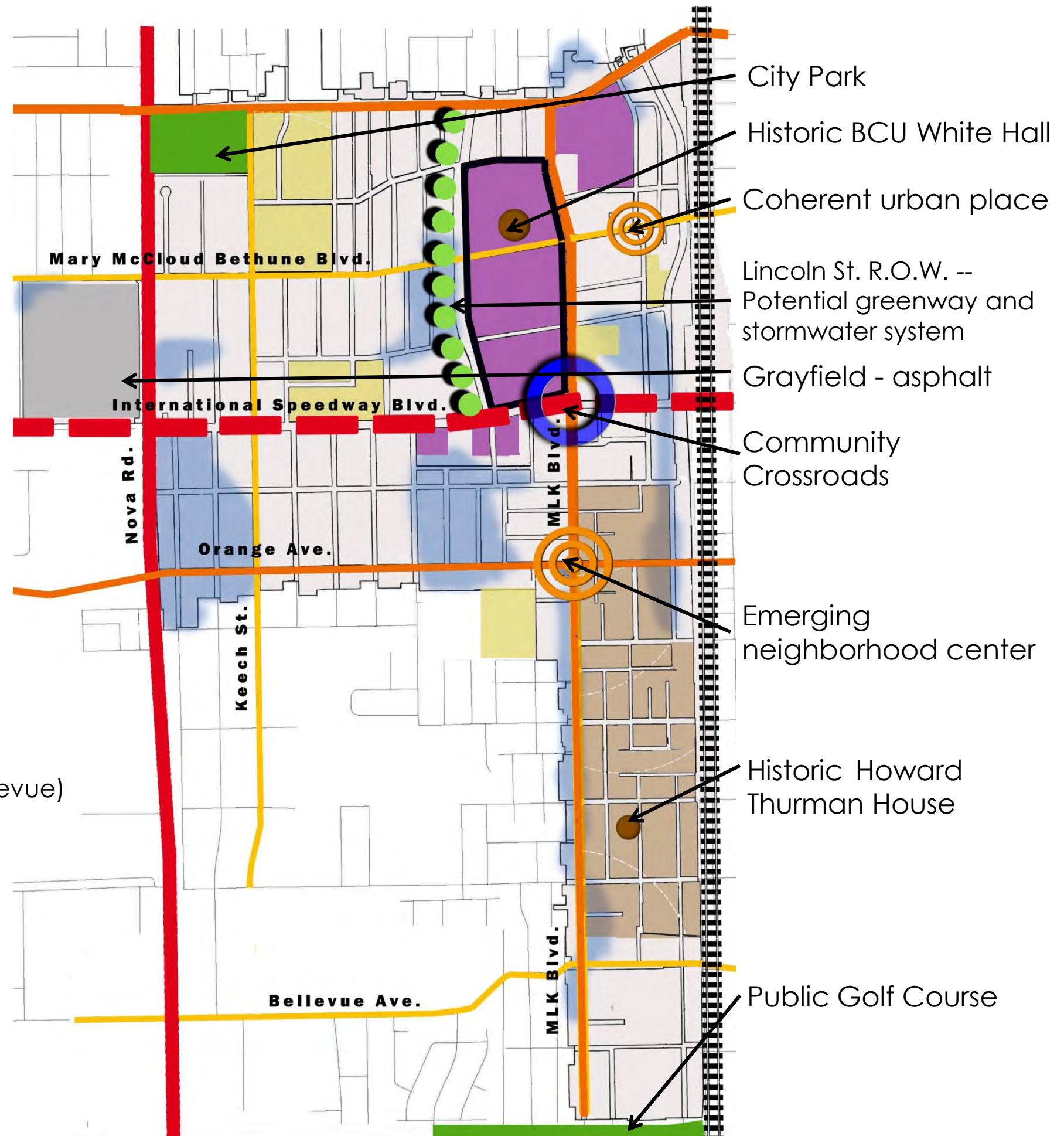
Plan Development Process :

1.4.1 Existing Conditions Analysis

The graphic analysis of the existing conditions includes GIS sources, site observations, and community workshop comments. The major arterials which connect Midtown to the beaches and the rest of Daytona Beach, present opportunities for economic growth; however, they are not pedestrian friendly. The primary district streets are integrated into the network of the city. The large parking lot (grayfield) at the N.E. corner of ISB and Nova Rd. is not the highest and best use its prominent location. The large un shaded area causes a heat island effect + reduces visible human activity from the street. More green spaces and parks were requested during the community workshops. There were also comments about both the safety of the neighborhood and pedestrian/bicycle safety. In addition to GIS maps indicating low areas in the district, during the workshops - the citizens indicated the location of flood-prone areas. Public housing areas are large and overly concentrated. Research shows diversity among income levels and zoning districts creates a lively, more economically stable community. The BCU campus is observed to be somewhat of an enclave. University efforts to provide a safe environment may have unintentionally weakened the historically strong ties to the community that the university enjoyed. A safe environment is desired by the community and the university; uniting the "town & gown" is an important first step towards reaching these mutual goals.

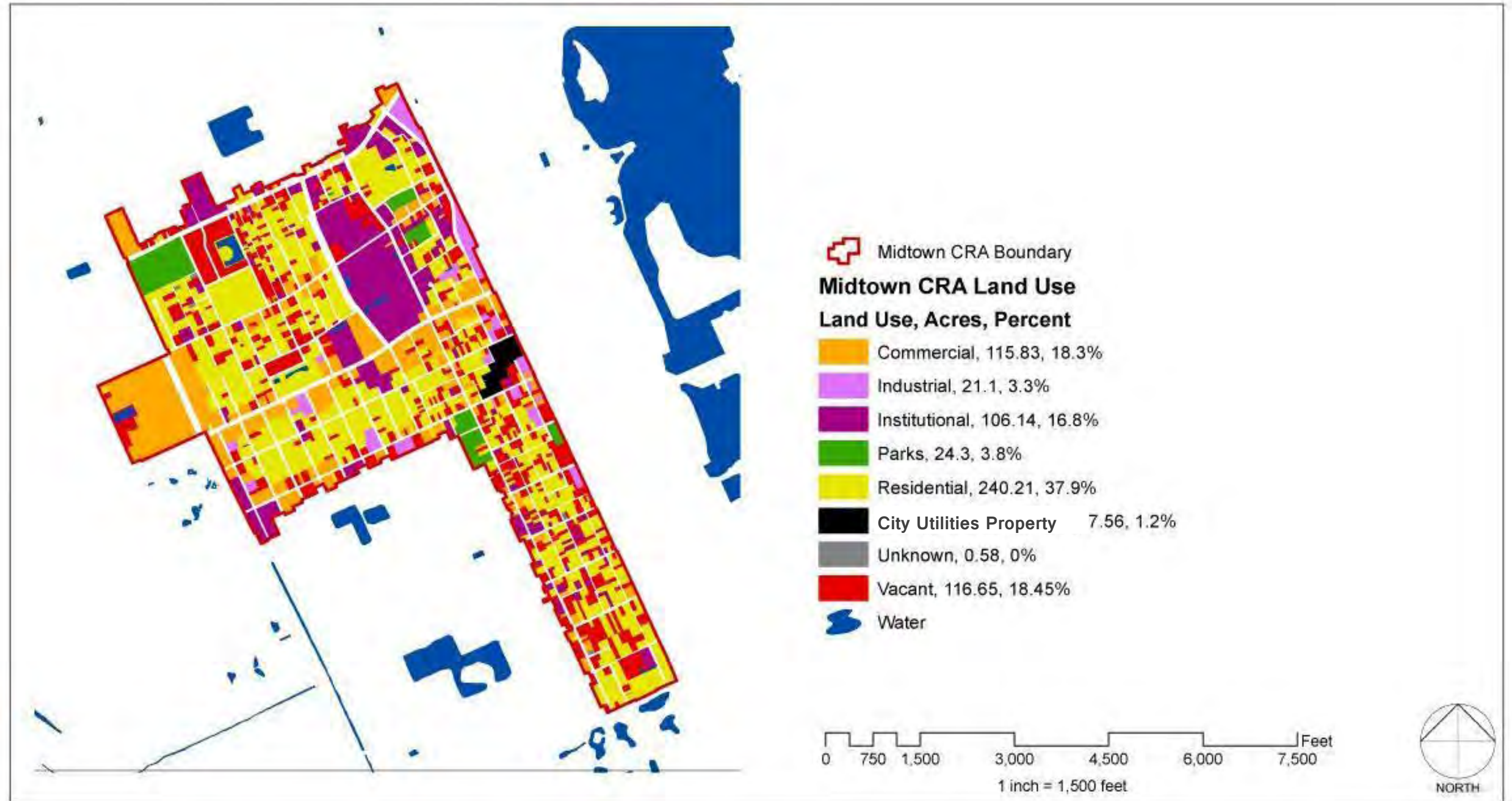
Legend

- - Major arterials/barriers
- - Primary District Streets (MLK & G.Engram)
- - Secondary District Streets (MMB, Keech, Bellevue)
- - Daytona Beach Housing Authority
- - Grayfield (large asphalt area)
- - Flood-prone areas
- - Public Green Spaces
- - Area with numerous historic structures
- - BCU, urban enclave



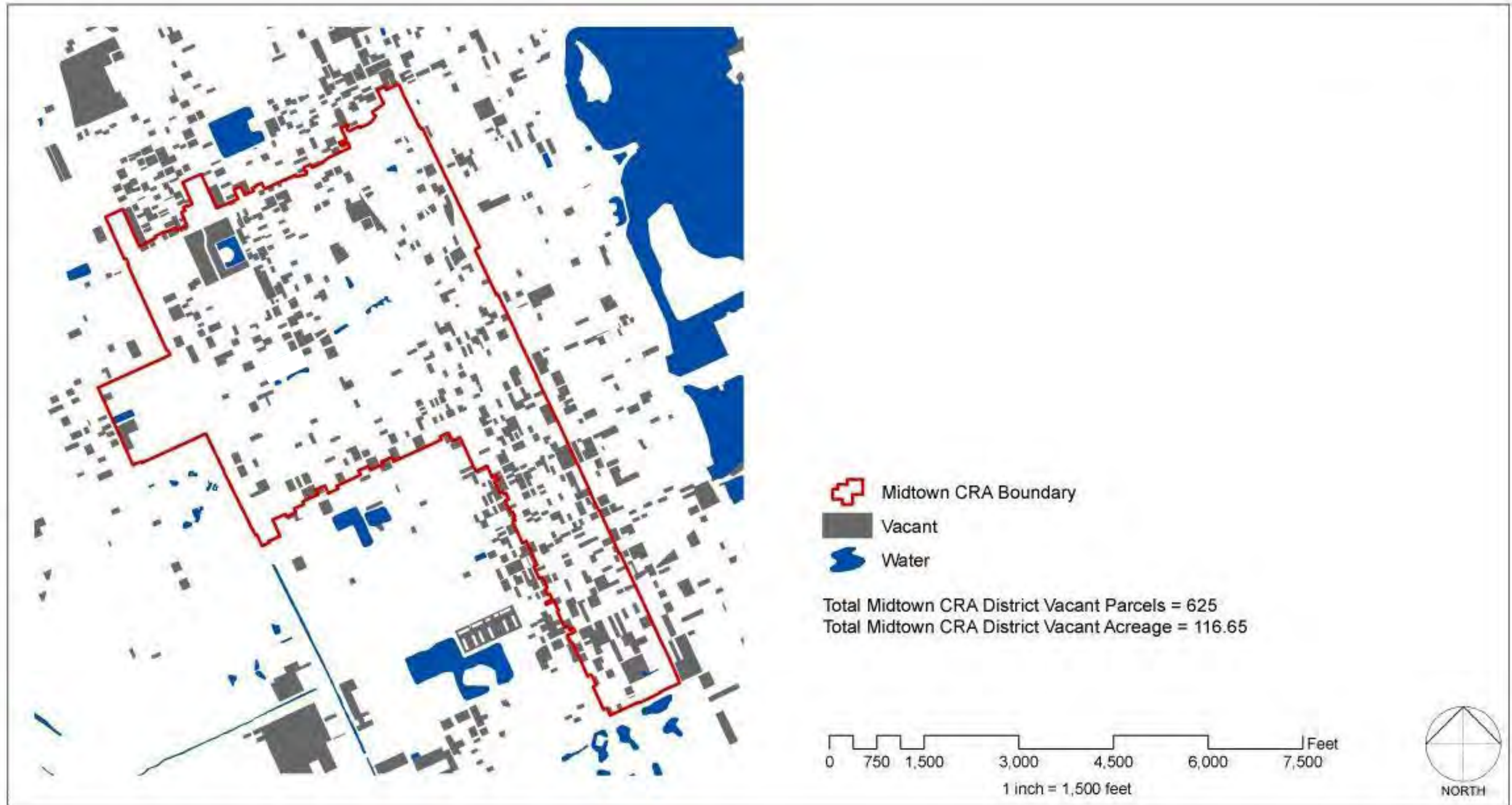
Plan Development Process : 1.4. 2 Midtown CRA – Existing Land Use Pattern

The current land use pattern does not follow a coherent plan. There are concentrations of lower density residential areas, the BCU campus and “strip oriented” commercial development. Most of the uses are separated and fragmented. There is not a hierarchy of public spaces with appropriate densities of uses / activities, and there is no identifiable center or “Main Street” in the community.



Plan Development Process : 1.4.3 Midtown Area Vacant Parcels Pattern

There are numerous vacant properties in the Midtown district. These “gaps” in the urban fabric depress property values and compromise urban connectivity; however, they also offer affordable vacant property that can accommodate growth and redevelopment. The vacant parcels include almost 120 acres of land that will add to the tax base when developed.



Plan Development Process : 1.4.4 Images of Midtown - Buildings & Places

"You do not want to encourage gentrification – the best approach is to mix the best of the old with a thoughtful introduction of new construction"

-- Craig Huffman,
Urban Design Research



The architectural landmarks and unique district character includes an eclectic mix of styles, materials, and color. These existing architectural patterns will inform the new design guidelines for the district.



Plan Development Process : 1.4.5 History - People & Places

"The movement of the Spirit of God in the hearts of men often calls them to act against the spirit of their times or causes them to anticipate a spirit which is yet in the making. In a moment of dedication, they are given wisdom and courage to dare a deed that challenges and to kindle a hope that inspires."

— Howard Thurman, *Footprints of a Dream*



South Beach Street, Daytona



South Bridge, Daytona



Mary McLeod Bethune (1875-1955)
Educator and Civil Rights Leader



Howard Thurman (1900-1981)
Minister, Educator, Civil Rights Leader



1874
The first motor race was held on Daytona Beach, Florida, in 1874. The winner was a man named...
K Vanderbilt, Mc Donald and Thomas on Ormond-Daytona Beach, Dec 20, 1874



Historic Cypress Street School

"I leave you love. I leave you hope. I leave you the challenge of developing confidence in one another. I leave you respect for the use of power. I leave you faith. I leave you racial dignity."

— Mary McLeod Bethune



The Howard Thurman House
Located at 614 Whitehall Street



Main Street Approach to the Beach



Old Faith Hall, Bethune Cookman College



Orange Avenue, Daytona

Historic images from *Daytona Beach Florida - A Post Card Tour*, Used for educational purposes.

2. Generating the Plan

Generating the Plan : 2.1 Vision Statement :

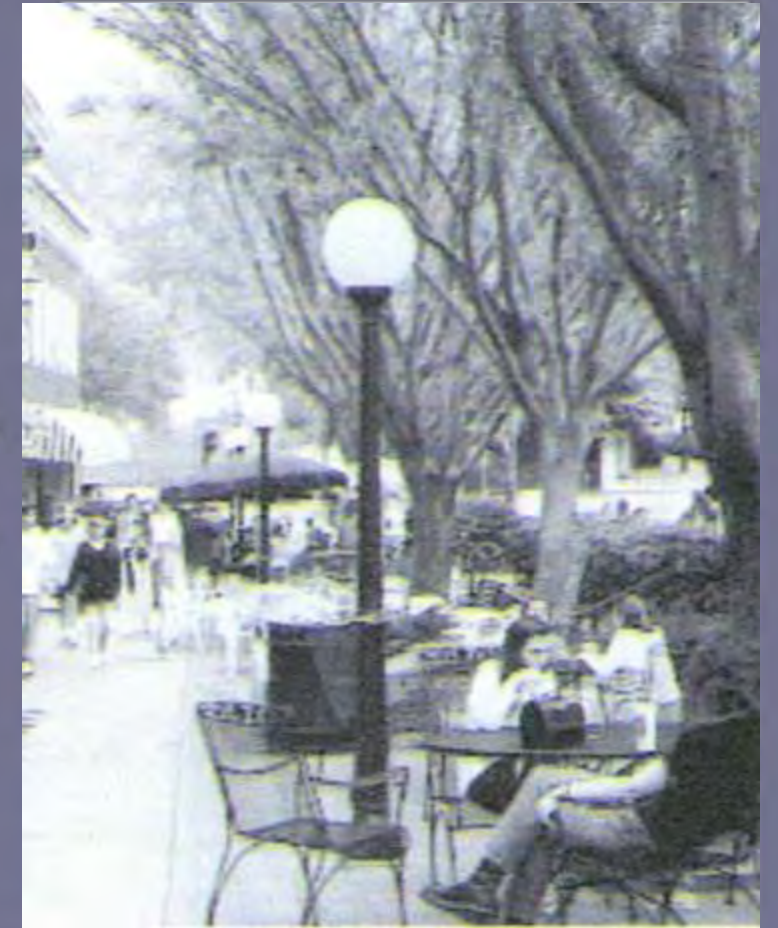
“To preserve the unique historic and cultural qualities of the African American Midtown District; while creating a new era for Midtown that is the model of an environmentally, economically, and socially sustainable community.”



Generating the Plan : 2.2 Planning Principles & Standards

These principles will guide the design of the Master Plan and will be used as performance standards to assess the effectiveness of the plan as it is developed and at 5, 10, and 20 year periods after implementation.

- **Improved, level of service for public facilities in the corridor, such as transit, parks, electrical, lighting, digital communication, water and sewer.**
- **Preservation of the unique history and character of the Midtown District**
- **An increasing share of the region's public and private investment in the Midtown District.**
- **An increasing percentage of home and business ownership in the district.**
- **An increasing percentage of the region's employment growth in areas within the district.**
- **An increasing personal income in every socioeconomic group.**
- **An increasing tax base.**
- **An increasing number of local residents participating in Midtown's community initiatives and activities.**
- **Existing places are enhanced and preserved. New places are unique and memorable.**
- **An increasing number of residential and commercial projects featuring pedestrian-friendly, energy-efficient, and transit-oriented design.**
- **Improved public safety in all areas, with implementation of Crime Prevention through Urban Design Standards.**
- **Improved public & private community service facilities**
- **Universal Design principles have been applied, and the community is accessible and allows aging in place.**
- **An increasing share of trips using transportation alternatives, such as public transit, biking, and walking, instead of single-occupant vehicles.**
- **A decreasing rate of per-capita consumption of resources such as water and electricity.**
- **Improved architectural standards and quality of construction.**



Generating the Plan: 2.3 Urban Design Principles & Patterns

In Midtown, there are a number of design patterns that inform the plan, but the six (6) that are essential in creating a successful urban community are:

1. **Preserve and extend the interconnected street / block network and establish small walkable blocks.**
2. **Build to the sidewalk; create defined streets and public spaces.**
3. **Locate surface and structured parking behind, on the side, below or above buildings.**
4. **Make retail and public storefronts / shops permeable and transparent on the ground level and include balconies / terraces that open to public streets, squares and courtyards.**
5. **Allow and encourage mixed use in horizontal and vertical development.**
6. **Establish a “Clear Center and Main Street” for the community.**

These simple design principles create a vibrant mix of compatible uses, provide adequate parking (while “taming” the “gray field deserts” of typical strip center parking), increase public safety due to more “eyes on the street” and promote a realistic market driven *incremental* development pattern.

Additional principles that guide the Master Plan and Development Standards are:

Enrich the Public Realm

The public realm includes all publicly owned streets, sidewalks, squares, parks, right of ways and other public open space and infrastructure. A good public realm encourages and facilitates walking. It allows and promotes a “public culture” of sitting & relaxing, of sidewalk cafes, neighborhood pubs & restaurants and other “3rd places” that compliment work and home. The proposed Master Plan includes a series of new public squares and monuments that will become the primary “outdoor rooms” of the community and enhance the existing Black Heritage Trail

Create a pedestrian friendly environment

In the Midtown Plan, vehicles and bicycles are accommodated and parking will be available and convenient, but pedestrians will come first. The district has the potential to be a successful pedestrian environment; however, there are a number of key items that need to be implemented for that to occur. These include: *sidewalk improvements & connectivity; access to transit; shade & protection from weather; visual interest, transparency & detail in the built environment; places to sit, meet, socialize and stage planned & informal community events.*



Generating the Plan: 2.3 Urban Design: Principles & Patterns

Address Human Scale

The standard metric of successful placemaking is sensitivity to human scale. Large blank building facades lack human scale and essentially communicate “you are not welcome here”. Successful urban environments address scale at every level. Buildings and places scaled to human movement include terraces, platforms, steps, niches, entries, canopies, windows, railings, details, materials, landscape and walking distances that relate to *people and their scale*.

Respect the scale & character of existing Historic District building patterns

A new project that becomes part of a neighborhood begins with an owner’s and designer’s appreciation of the strengths of the existing context, plus an understanding of the intent of the Development Standards for a particular location. Neighborhood context is generally strong in Zone 5: Residential Historic Infill, Zone 6: Residential and Zone 8: University Transition. In other zones and areas of Midtown, a new character will emerge as the Master Plan guides new development.

Design for the Climate

The Development Standards include basic Green Building strategies that address the coastal Florida climate. Traditional and contemporary architectural elements include: arcades, deep porches, awnings, canopies and green trellis walls for shade; and balconies, tall windows, French doors, shutters and light shelves for *light and ventilation*.

Design for Everyone

Universal design will inform public signage, seating, transit and mobility. Children, elderly, and differently abled residents will be considered in the design of new public spaces.

Safety and Crime Prevention

Public areas will be safer through the application of CPTED (crime prevention through environmental design) principles. **(CPTED)** is a multi-disciplinary approach to deterring criminal behavior through environmental design. CPTED strategies rely upon the ability to influence offender decisions that precede criminal acts. CPTED is based upon the theory that the proper design and effective use of the built environment can reduce crime, reduce the fear of crime, and improve the quality of life. Built environment implementations of CPTED seek to dissuade offenders from committing crimes by manipulating the built environment in which those crimes proceed from or occur. The three most common built environment strategies are *natural surveillance, natural access control and natural territorial reinforcement / maintenance*.



Midtown Masterplan

Generating the Plan:

2.4 Streets & Transit

Transit & cycling:

- The existing bus loop will be maintained and extended to connect all 5 Neighborhood Centers as they become more established.
- New transit stops and shelters will be accessible, use green technology for lighting and be designed to educate & celebrate the civil rights history of the Midtown District.
- The proposed Amtrak Station (located at Magnolia Ave.) offers an opportunity to connect the district to the larger regional and national rail transit system.
- The interconnected street network and urban scale create an ideal environment for bicycle transit. Since traffic is dispersed in the grid, most of the streets work well as shared lanes for motorists and cyclists; however, certain streets need to be modified to include dedicated bike lanes, these include: *ISB, Nova, G. Engram, MMB Blvd., Orange Ave., Bellevue Ave. and certain portions of MLK Blvd. and Keech St.*

Street, Sidewalk & Infrastructure Improvements:

As funds become available for street and R.O.W improvements – design elements that support “*streets for people*” will be included throughout the district.

These elements include:

- pedestrian “bumpouts” at intersections & improved crosswalks
- wider resurfaced sidewalks with accessible curb transitions
- curb & gutters with improved drainage
- parallel & diagonal (on wider R.O.W.s) street parking
- street trees for shade, scale & traffic calming
- pedestrian oriented street lights with themed signage banners that provide orientation for historic sites & *Heritage Walk* locations
- improved vehicular & pedestrian signalization

Transit - Midtown

People Oriented Streets

Neighborhood streets are the lifeblood of cities. Like arteries that sustain life in surrounding cells, our streets sustain life in our neighborhoods. Simply put, if our streets are not healthy, then our neighborhoods are not healthy.

So what are healthy streets? First, a street is all of the public space that includes the car lanes, median, and sidewalks right up to the private property line. Healthy, or “complete streets”, are designed to serve the needs of all the people who use them—parents pushing strollers, grandmothers, children walking to school, people driving to work, bicyclists, people using wheelchairs, and people taking the bus. They are public spaces for people.

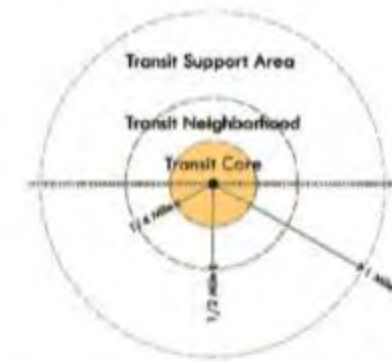
Build on Strong Connections & Reconnect Dead Ends



Bus Route



C.O.D.B. Future Street Improvements



MLK & Orange Looking North



Existing Conditions



After Public Space Improvements

- Underground Utilities
- Wider Sidewalks
- Street Trees
- Special Paving

Generating the Plan: 2.5 Parks & Green Space

Currently only 3.8% of the land in the Midtown CRA is dedicated public green space and the majority of it is located north of ISB.

Proposed parks & public spaces:

- Midtown Center Square
- Neighborhood Center Squares
- Howard Thurman Community Gardens
- Lincoln Street Greenway
- Dickerson Center Park

Parks, Recreation & Public Space



Public Space Initiatives:

- Create an interesting and attractive public realm: recreation spaces, open-air markets, passive parks, bus-stops, bicycle storage, dog park, bicycle trails, public water features, monuments, decorative street lights, street trees, landscaped boulevards
- Shaded sidewalks & pedestrian lanes
- Plazas with Water features that children can play in.
- Parks for organized sports and informal "pick-up games" and playground games
- Increase the Parks & Open Space from 3.84% to 10% +/-

"Urban redevelopment is not just an exercise in beautification, **it is an economic development engine.** Success is based on creating a strategy for improvement calibrated to the local economic opportunities that the market can deliver."

— Daniel & Karen Parolik, AIA + Paul Crawford FAICP,
East Bay Area

Shaded Boulevards

A great street (especially in Florida) is shaded by urban street trees. In addition to shade, street trees provide human scale, filter urban air, promote traffic calming, reduce traffic accidents due to reduced speed, save cost of drainage infrastructure due to absorption of the first 30% of runoff, increase business activity and property values, and provide a more pleasant walking environment. Pedestrian scaled street lights are placed below the tree canopy so the illumination is not blocked by the trees, or smaller trees are used (As shown in the proposed Orange Ave. Section).



Future Example of Intl. Speedway Blvd.



Future Example of MLK Blvd. & Orange Ave.



Typical Residential Street

Generating the Plan:

2.6 Community Gardens & Public Markets

Proposed public gardens & markets:

- Howard Thurman Community Gardens
- Lincoln Street Greenway

“Successful public markets and community gardens help grow and connect urban and rural economies. They encourage development, enhance real estate values, increase the tax base of adjacent land and keep money in the neighborhood. Public markets also offer “low risk” business opportunities for local vendors and entrepreneurs.”

The Project for Public Spaces

Community Gardens

Midtown's Community Gardens will make vacant properties into productive neighborhood centers with economic and health benefits. A Farmers Market will be located near the largest garden as a focal point for the community.



Generating the Plan: 2.7 Universal Design: Designing for Everyone

Universal Design increases opportunities & life choices for a pluralistic population including those often underrepresented - people who have disabilities, health conditions, and low incomes; & elderly people and children. The Daytona Beach Midtown Master Plan is an opportunity for universal/inclusive design to improve the lives and welfare of Midtown's citizens and will allow maximum mobility and access.



Aging in Place

Intergenerational engagement **benefits the entire community.**

Mixed-use neighborhoods and intergenerational transportation programs can **help connect older adults to the community** at large.

Bringing people of all generations into the planning process ensures that everyone's voice is heard.

Friendly, intergenerational housing should be available to older adults as **an alternative to retirement and nursing homes.**

“ More than 40 percent of older adults say that they have at least one problem with their home that is in need of physical repair.”

—Sandy Markwood, Executive Director, National Association of Area Agencies on Aging

3. Midtown Redevelopment MASTER PLAN

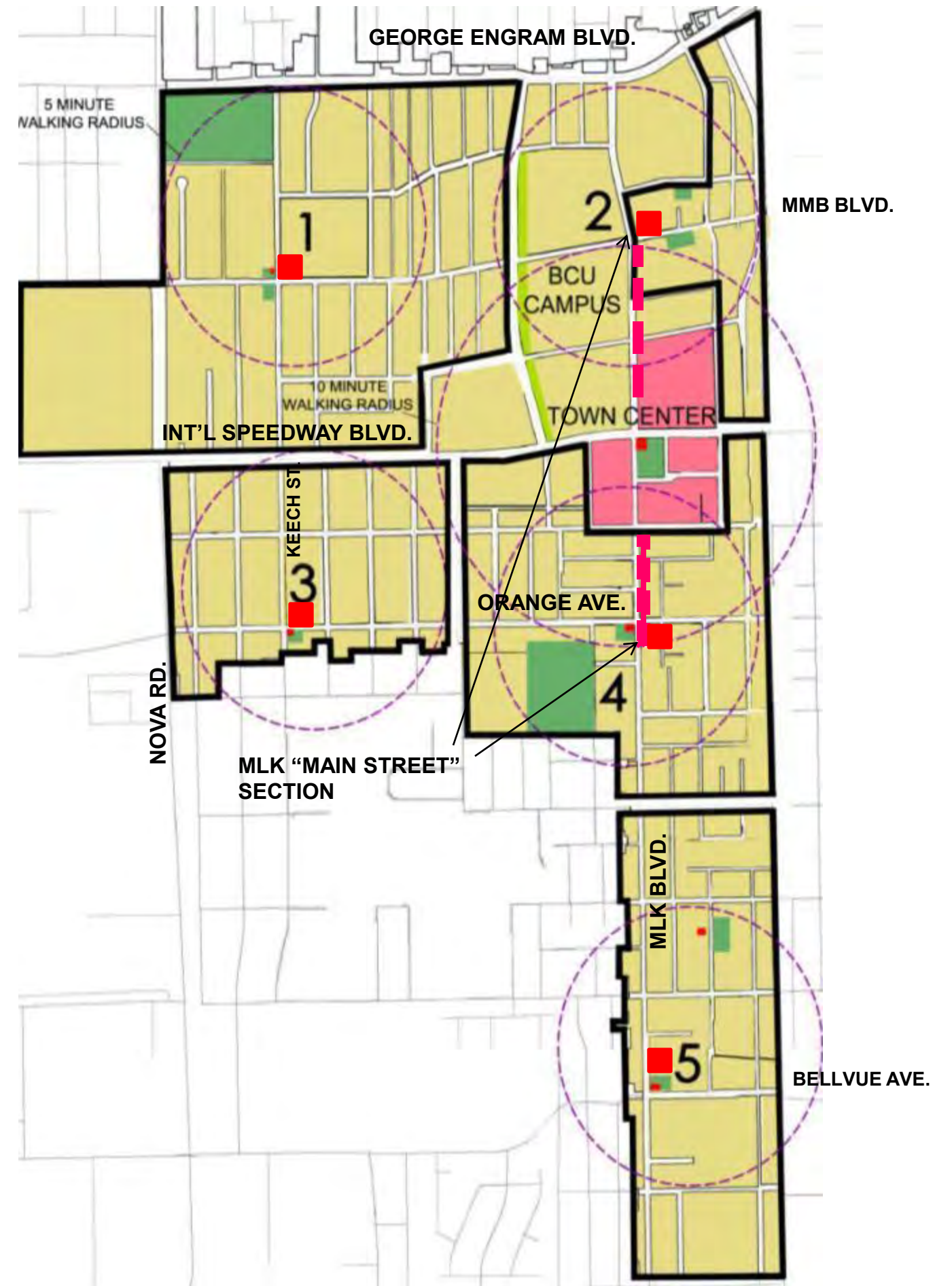
Master Plan: 3.2 Neighborhood Plan

The district has been divided into five (5) sub-districts or “neighborhoods”, and a “Town Center” located at the intersection of ISB AND MLK Blvd. The Town Center connects with the “Main Street” portion of Martin Luther King Blvd. (between Mary McCloud Bethune Blvd. & Orange Ave.)

The neighborhoods are organized around “5 neighborhood centers” and 5 minute pedestrian (1/4 mile) walking distance areas. The neighborhoods will have identifiable centers with a mix of uses, neighborhood services and transit stops. The neighborhood centers are located based on the intersections of the primary streets and avenues in the CRA District. Over time, each neighborhood will develop a unique character within the larger Midtown district. Their size is based on the 1/4 mile walking shed and typically range between 100 and 150 acres. The new Land Development Regulations and future land use patterns are based on this neighborhood structure. The highest densities and intensity of uses will be at the Town Center Square and to a lesser degree at the five neighborhood centers.



Initial Concept:
Sketch Study



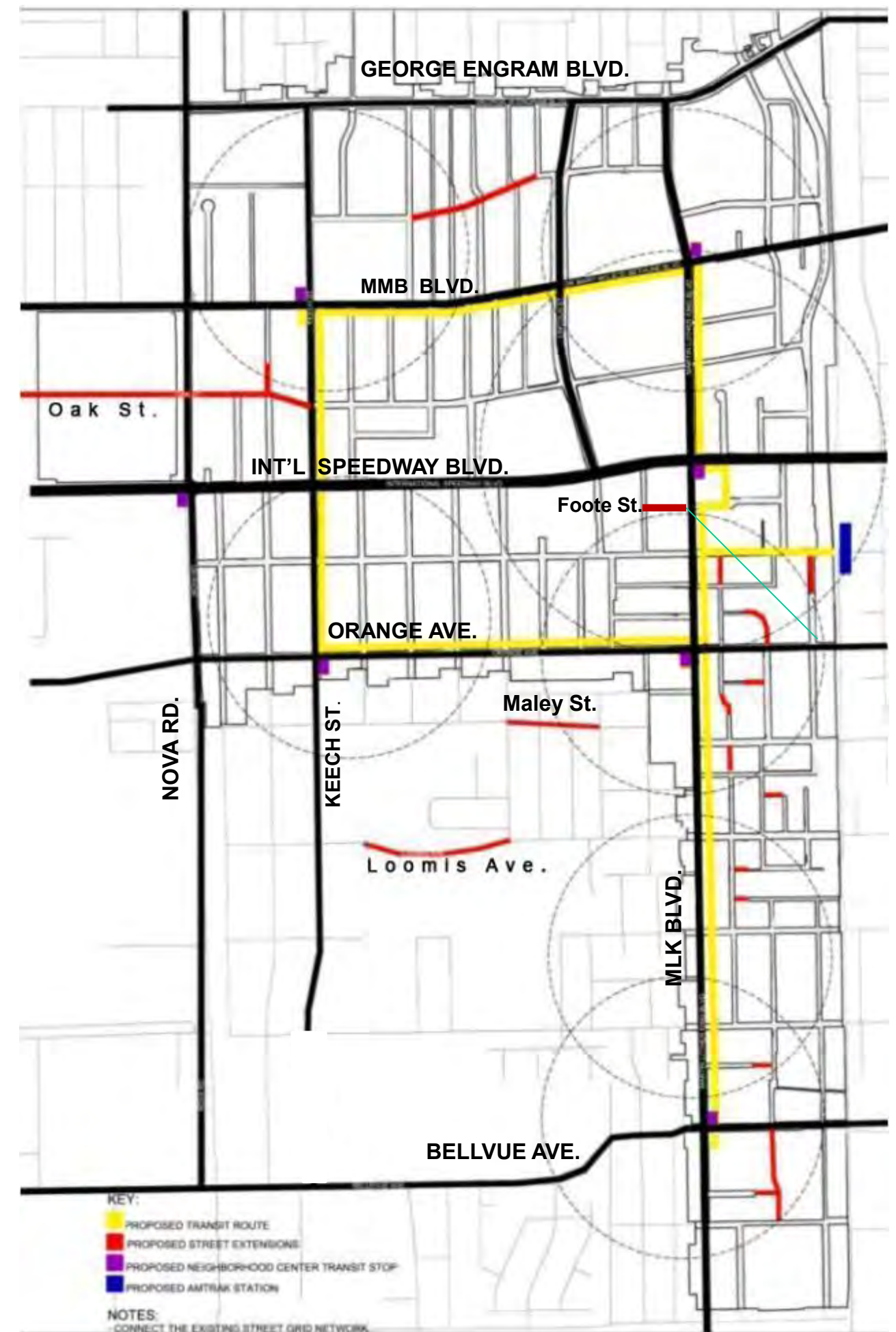
Master Plan: 3.3 Streets & Transit Plan

One of the strengths of the Midtown District is the interconnected street network of small blocks. This armature has allowed a pedestrian scaled development pattern to emerge over time. The Master Plan will preserve the grid and extend certain streets to address gaps and barriers in the network.

The plan focuses on the intersections of 5 primary streets: *MLK Blvd., Orange Ave., Keech Street, Mary McCloud Bethune Blvd. and Bellevue Ave.* Five (5) of these intersections will form Neighborhood Centers with public squares / transit stops.




Street extensions & connections include:

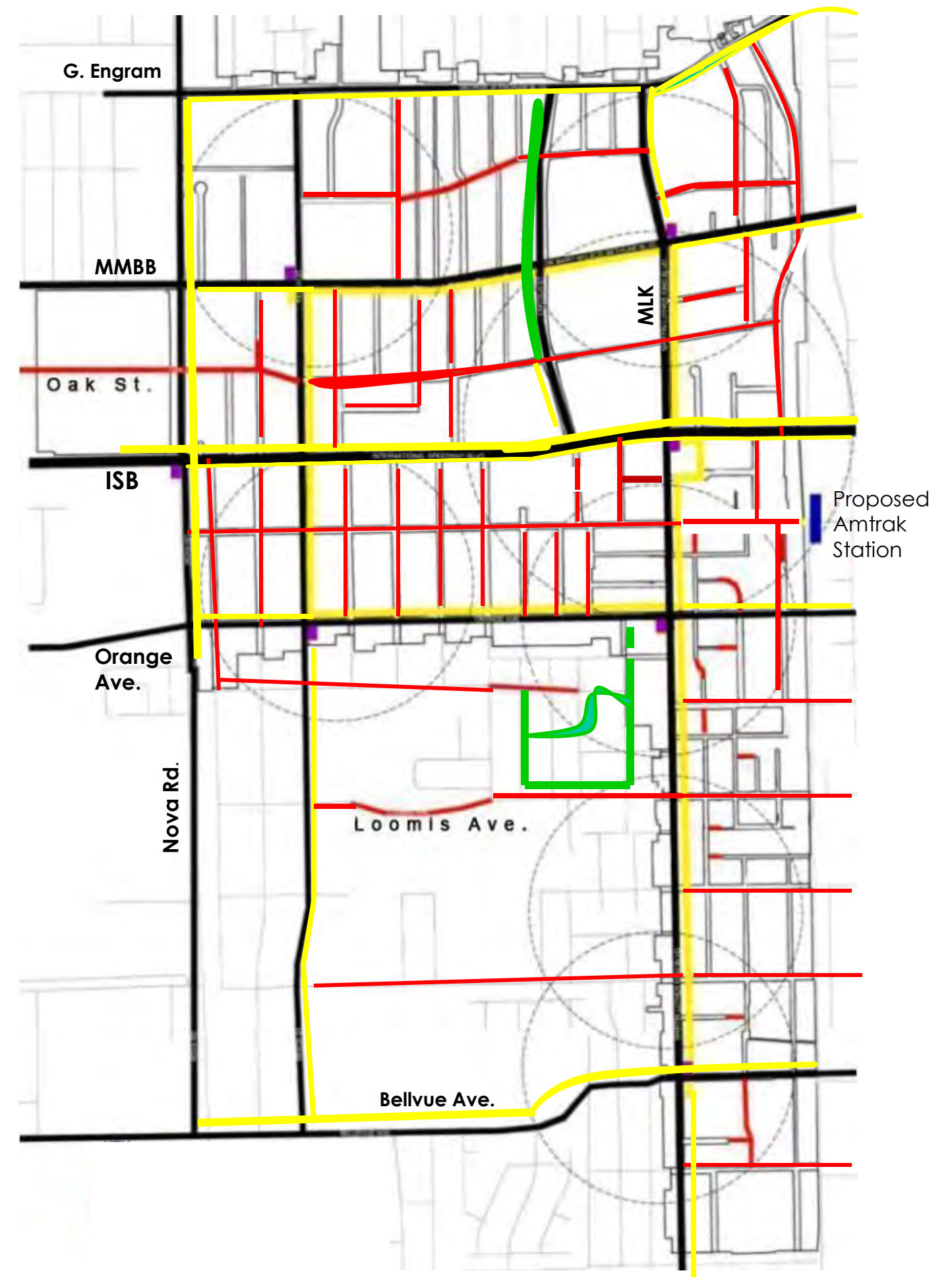
- Oak Street (initially west to Nova Road and in a future phase – west through the redesigned Daytona Mall property).
- Loomis Ave. (connecting the separated portions of the avenue on the western edge of the district).
- Maley Street (at the north edge of the proposed Dickerson Center recreational park expansion).
- Foote Street (refer to the Midtown Center Redevelopment Plan).
- Connection & removal of dead end streets in the historic residential district located south of ISB & east of MLK Blvd.



Master Plan: 3.3.1 Bicycle Lane & Trail Plan

The interconnected street network and compact scale of Midtown is an ideal environment for cycling. Bicycle use is relatively high compared to other areas of the city. To support the current bike use and provide additional opportunities for cycling and improved safety, the Master Plan includes 3 types of bike transit lanes:

1. 4' wide bike lanes on major streets (one for each direction of travel). This type will be located on ISB, Nova Rd., G. Engram, Mary McCloud Bethune Blvd., Orange Ave., Keech St., Bellvue Ave. and portions of MLK Blvd. 
2. 5' wide bike trails (separated from Lincoln Street) in the Lincoln Greenway and around the expanded Dickerson Center Park. 
3. The interconnected grid of secondary and minor streets. Streets will have appropriate signage indicating that cyclists may ride in the vehicular lane. 



Master Plan: 3.4 Parks & Street Tree Plan

Currently only 3.8 % of the land in the Midtown CRA is dedicated public green space and the majority of it is located north of ISB. The plan includes new parks and green spaces that address recreational, environmental, infrastructure and public space needs. All of the proposed parks, gardens and public spaces are in existing street right of ways and publically owned properties.

Proposed parks & green space improvements:

- 1. Dickerson Center Park** – A new public park that includes both active and passive recreational uses & facilities. The proposed location is west of the existing Dickerson Community Center on existing Housing Authority property. Land acquisition through “land swaps” between the city of Daytona Beach and the Housing Authority would be required for the project.
- 2. Town Center Square @ ISB & MLK Blvd.** – The Public Square will be the cultural and commercial heart of Midtown. It will include shaded seating + gathering areas with fountains, public art and Black Heritage Trail destinations.
- 3. Lincoln Street Greenway** – The wide Lincoln St. right of way along the west side of the Bethune Cookman campus offers the opportunity to create a landscaped greenway that includes stormwater / drainage infrastructure and a landscaped bicycle / pedestrian path that will form part of the Black Heritage Trail. The greenway will define and enhance the western edge of the university and add an attractive linear park to the residential area west of campus. Portions of the greenway may include community gardens that are planted and maintained by neighborhood + university organizations. Diagonal street parking will be added to the BCU side of the street to enhance pedestrian activity and promote environmental safety.
- 4. Neighborhood Center Greens** – The neighborhood greens are small parks based on the scale and character of the existing Huger Park (located at the southwest corner of MLK Blvd. & Orange Ave.) The new zoning overlay establishes NC-MU (Neighborhood Center – Mixed Use) around each green. This will activate the small parks with a mix of complimentary uses including appropriate residential building types. Transit stops, seating, fountains, shade trees and connections to the Black Heritage Trail will be included in all of the neighborhood parks. .
- 5. BCU traffic calming public art circle** - The center of the BCU campus is bisected by Mary McCloud Bethune Blvd. A small traffic calming circle with public art will enhance the public space and become a part of the Black Heritage Trail.
- 6. Street Trees** – Native Florida trees will be added to all major streets as part of a phased street improvement program.
- 7. Proposed Loomis Pond stormwater infrastructure** – A recent stormwater study identified this location as a potential site for an 11 acre pond. More input from the neighborhood is needed before the stormwater plan is implemented.
- 8. Howard Thurman Gardens** – The city property located across the street from the historic Howard Thurman House offers an opportunity to develop a neighborhood garden and an elderly housing community.



Master Plan: 3.5 On-Street Parking Plan

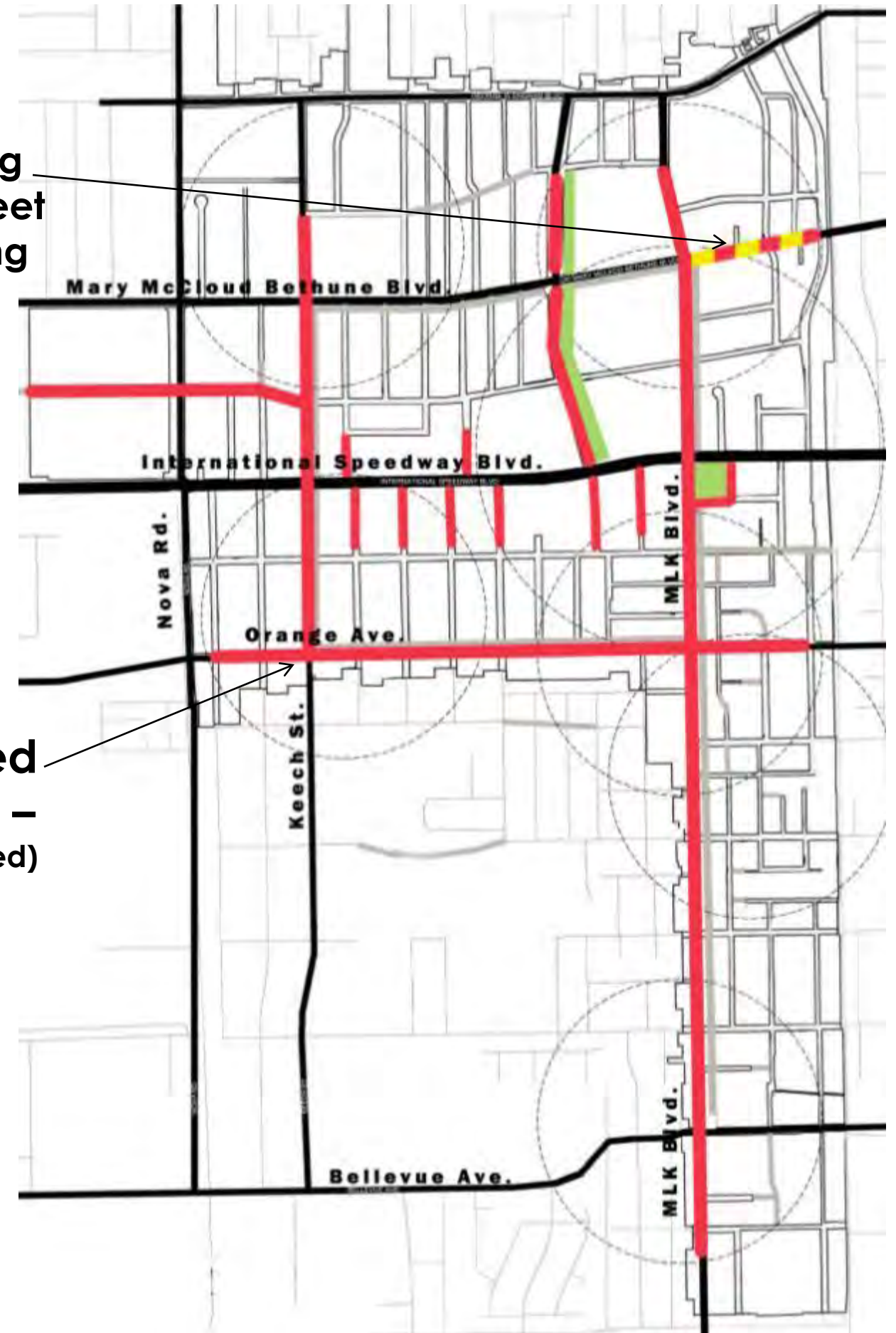
On street parking is an essential element of a successful urban plan. Diagonal or parallel parking increases pedestrian safety; reduces crime due to the increased activity of people parking and walking to / from their cars; and supports business development. To increase the development density to urban standards, an incremental on street parking plan will be adopted that fits in current right of way widths and meets safety standards.

PROPOSED ON-STREET PARKING COUNTS

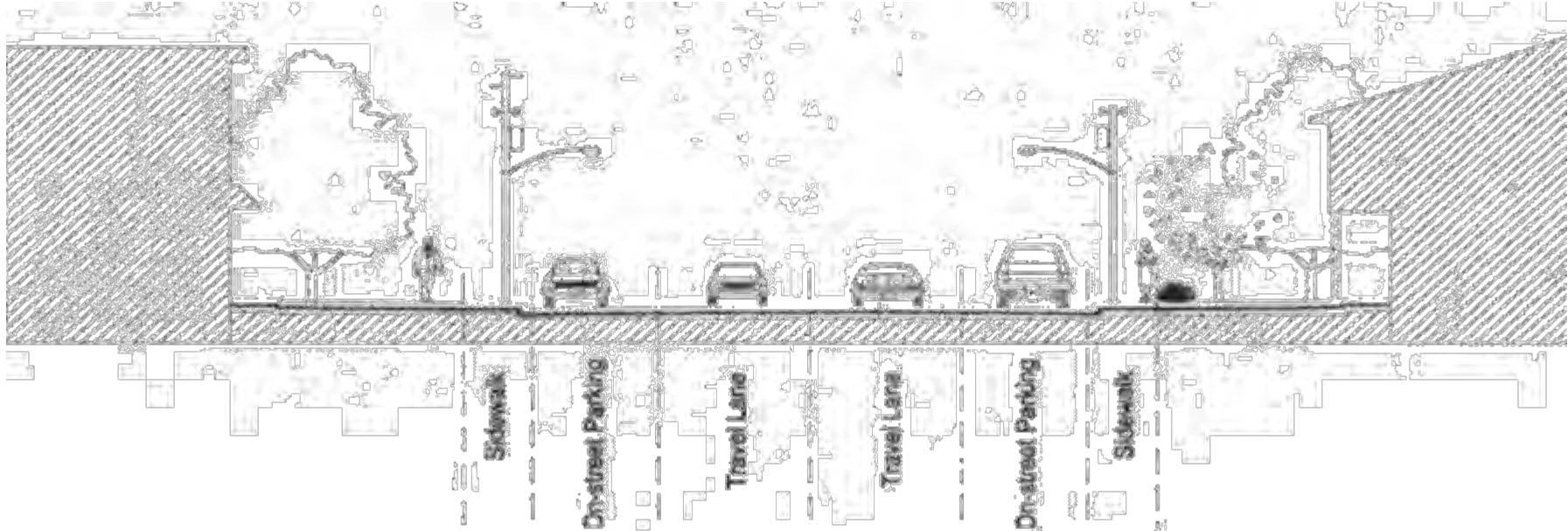
STREETS WITH DIAGONAL PARKING one side only	LINEAR FEET	PARKING SPACES
Lincoln – north (ISB to G. Engram)	2640	248
STREETS WITH PARALLEL PARKING – N/S STREETS one side only		
Harney (Oak St. extension)	500	20
Caroline St. (Magnolia to Oak St. extension)	900	34
Keech St. – north (ISB to MMB Blvd)	1500	56
Keech St. – south (ISB to Magnolia)	1500	56
Adams (Magnolia to Oak St.)	900	34
Reva (ISB to Magnolia)	400	16
Franklin – north (ISB to Oak)	450	18
Franklin - south (ISB to Magnolia)	400	16
Lockhart (ISB to Magnolia)	400	16
Lincoln (ISB to Magnolia)	450	18
MLK – north (ISB to G. Engram)	2200	88
MLK – south (ISB to Bellevue)	3900	156
STREETS WITH PARALLEL PARKING – E/W STREETS one side only		
MMB Blvd. (Keech to Lincoln)	1440	57
New Oak St. extension (Nova to Keech)	600	24
Orange Ave. (Keech to Marion St.)	2400	96
TOTAL		881

Existing
on street
parking

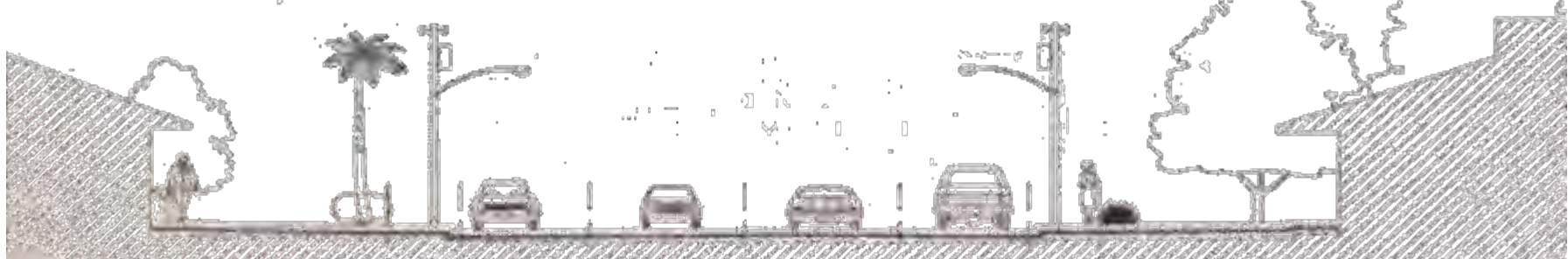
Proposed
parking –
(shown in red)



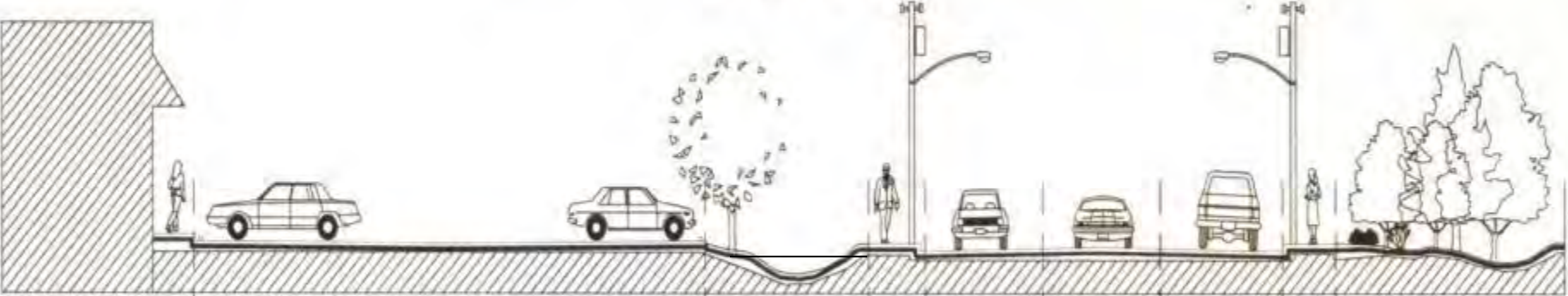
Master Plan: 3.5.1 On-Street Parking Street Sections



Mary McCloud Bethune Blvd (Extend existing cross section west to Nova Rd.)



Keech St



Parking Lot

Swale & Bike Lane

Sidewalk

Travel Lane

Turn Lane

Travel Lane

Sidewalk

Travel Lane

Swale

Orange Ave

Master Plan: 3.6 - Black Heritage Trail, Public Spaces & Monuments Plan

There are a number of significant historic sites located throughout Midtown. When the proposed Midtown Center Square is established at the intersection of ISB & MLK Blvd.; It will be an excellent location for a new African American Heritage Center. The center will document and present the important contributions to Civil Rights by the Daytona Beach Midtown Community, Eatonville and central Florida. The trail will be marked by themed signage, street lights, pavement colors and displays that link all of the heritage sites.

The images and Heritage Sites were provided by J. Benderson & the C.O.D.B Redevelopment Dept.



Midtown Redevelopment MASTER PLAN: 3.1 Twelve Actions

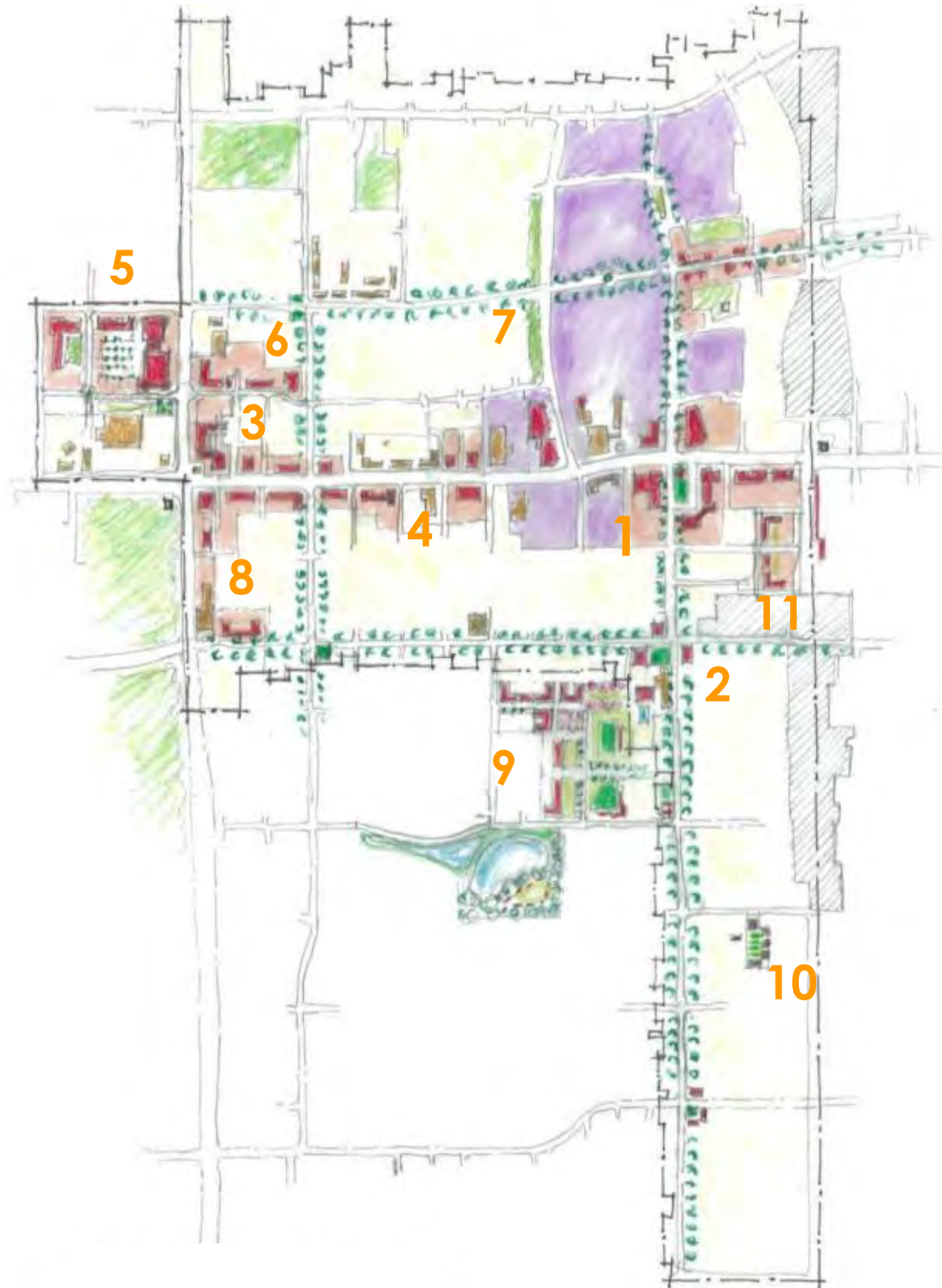
- 1) Establish the “Town Center” and “Five Square” organization as shown in the Neighborhood Plan.
- 2) Establish 5 (five) distinct neighborhoods within Midtown, each with a unique character & identifiable center that are based on the scale of a 5 minute walk.
- 3) Protect & infill the existing residential areas.
- 4) Connect, & extend the street network as shown in the “Streets & Transit Plan”.
- 5) Create a Midtown “Town Center” at the intersection of ISB & MLK and establish MLK Blvd. as a “Main Street” (between Mary McCloud Bethune Blvd. on the north & Orange Ave. on the south) with heritage exhibits, public art and a lively mix of entertainment, cultural, retail, residential and educational uses.
- 6) Create entitlement & property development incentives for the ISB corridor & all public property that is being offered for private development.
- 7) Expand the Dickerson Community Center and add new recreational & aquatic facilities adjacent to the Dickerson Center. Add multi generational work force and subsidized housing around & near the new parks & green spaces. (The proposed parks & housing would be developed southwest of the Dickerson Center on the existing public housing property between Hudson & Lockhart St.)
- 8) Create a new transit plan based on the Five Square plan organization.
- 9) Establish a Community Garden & infill residential / assisted living project on Whitehall St. near the historic Thurman House.
- 10) Adopt a Form Based Code zoning overlay with Development Standards & Design Guidelines that are based on the unique characteristics of the Midtown District.
- 11) Create design standards that are tailored to the scale & character of each neighborhood. Standards will include Universal Design & CPTED (crime prevention through environmental design) principles.
- 12) Coordinate & prioritize proposed infrastructure & street improvements based on the Master Plan.

4. Redevelopment Proposals

Redevelopment Proposals: 4.1 Redevelopment Focus Areas Plan

Redevelopment Sites with proposed uses:

1. **Midtown Center at ISB & MLK:** a mix of university facilities, mixed use, entertainment & cultural / heritage destinations
2. **Huger Park – typical neighborhood center**
3. **ISB & Nova (vacant car dealership & restaurant), potential uses:** hotel / restaurant, grocery or pharmacy
4. **ISB infill:** misc. commercial & mixed use
5. **Daytona Mall, potential uses:** large scale discount/grocery, multi screen theater, Activity Center retail & commercial
6. **Oak St Extension:** market rate & work force/student housing
7. **Lincoln Greenway:** landscaped greenway that includes stormwater / drainage infrastructure and a landscaped bicycle / pedestrian path
8. **City property / police station:** mixed use residential, student housing, magnet school for the arts
9. **Dickerson Center Expansion:** new community meeting center, gym & aquatic center; passive & active recreational fields; new work force, affordable & senior housing
10. **City property at the Thurman House:** community garden & small park surrounded by infill & senior housing
11. **City utilities site:** commercial & eco industrial uses

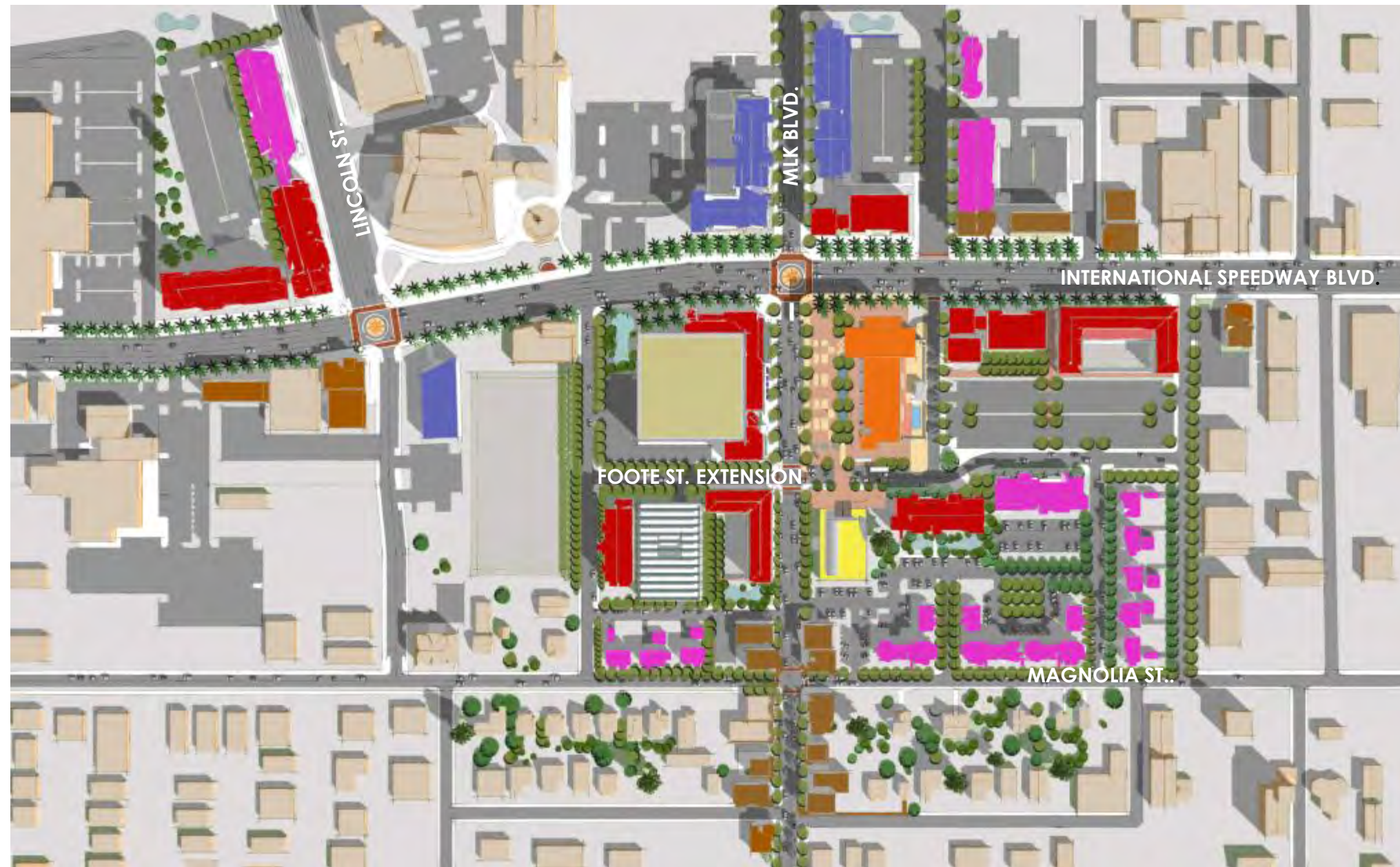


Redevelopment Proposals: 4.2 Midtown Center at ISB and MLK - Site 1



This rendering of the Midtown Center at the intersection of ISB and MLK, shows the scale and uses that are included in the zoning documents. The plaza functions as a public space to help stitch the university into the fabric of the Midtown community. The dense commercial at ISB transitions to smaller scale commercial/residential at the Foote St. extension, then transitions down to townhomes and single family homes at Magnolia Street, complementing the existing homes.

Redevelopment Proposals: 4.2.1 Midtown Center at ISB and MLK - Site 1



Proposed Uses

Legend

- Midtown Square
- Mixed-use: Retail, Office, & Residential
- Hotel, Restaurant, & Retail
- African American Heritage Museum
- Theater or Grocery with Retail
- Future University Facilities
- Parking Garage
- Small Commercial
- Townhomes, Apartments, & Duplexes
- Existing structures



Redevelopment Proposals: 4.2.2 Midtown Center at ISB and MLK - Site 1



Development Phases

- PHASE 1
- PHASE 2



PHASE 1: estimated gross square feet = 550,000; additional parking = 1,200 spaces
PHASE 2: estimated gross square feet = 650,000; additional parking = 2,000 spaces
TOTAL GROSS SQUARE FEET = 1,200,000 (estimate) TOTAL PARKING = 3,200 SPACES
375 sq. ft. average per space

Note: Town Center Plaza will be developed after property acquisitions & infrastructure improvements are in place.

Redevelopment Proposals: 4.2.3 Midtown Center at ISB and MLK - Site 1



View through plaza towards movie theater/retail at MLK Blvd.



Looking north at MLK; street café left, plaza with hotel right.

The birdseye view looking northeast from Foote St. extension at MLK Blvd over the new plaza includes a large bus shelter and bicycle racks with a bus loading zone at the southern edge of the plaza. The plaza links the African American museum (bottom right), the hotel with restaurant/retail, retail/office at ISB (top left) and the movie theater with entertainment /retail(bottom left.) Parallel street parking and shaded surface parking (top right) serve initial phases, however, parking garages could be located at surface lots, as demand increases in the future. This conceptual design illustrates the Zone 1 Midtown Center Mixed Use standards.



(Zone 1 Midtown Center Mixed Use)

Redevelopment Proposals: 4.2.4 Midtown Center at ISB and MLK - Site 1



Birdseye view looking southwest over Midtown Center at International Speedway Blvd. and Martin Luther King Blvd.



Redevelopment Proposals: 4.2.5 Midtown Center at ISB and MLK - Site 1



Looking east into new Plaza. Note street trees and street lights; awnings and umbrellas provide shade at street café.



New buildings shown in white, existing buildings tan. Significant new buildings have been shown with color for emphasis.



Trees, awnings, umbrellas, and deep arcades provide shade at the plaza. A mixed-use project is a key factor to the success of the plaza.



The water runnel through the plaza is a conceptual axial link between the university and the community. Paving patterns are based on textile patterns of African American quilts.

Midtown Masterplan

Redevelopment Proposals: 4.2.6 Midtown Center at ISB and MLK – Site 1



Looking northeast from Magnolia St. towards proposed townhomes and apartments.



Looking west over rooftops of new apartments and African American Museum at Foote St. extension to Movie/Entertainment complex.



View north at MLK; existing commercial left. Note new street trees, street lights, distinctive cross-walks & new commercial buildings.



Looking south at ISB and MLK.

Midtown Masterplan

Redevelopment Proposals: 4.2.7 Midtown Center at ISB and MLK – Site 1



View looking east at ISB; new university mixed-use and offices complement existing the university presence along ISB. Large scale street trees with less foliage are appropriate at ISB because they allow open views to the buildings and are a better fit with the 3 to 5 story scale. Also, the street lights are larger on ISB and feature distinctive Midtown and University banners. The existing decorative cross street design is picked up throughout the area by using the same colors at additional significant cross-walks.



View south over ISB; new Hotel right, new commercial/retail left.



(Zone 1 Midtown Center Mixed Use) Looking southwest over ISB.

Redevelopment Proposals: 4.3 Midtown Center at ISB and MLK – Site 1 Town Square Option



An alternative option for the Midtown Center Plaza is to create a larger Town Square. A larger town square would provide sufficient space for festivals and community events in a central location with high visibility to the Daytona Beach area. The conceptual design features an amphitheater, passive grassed areas, paved walks, public art, benches along an allé of manicured trees, and a clock tower. The Hotel is located adjacent to the square. All other proposed uses, like the Movie theater and African American Museum remain the same as in the Plaza concept. This concept assumes denser populations and a higher number of visitors to the area, therefore a four level parking garage is shown behind the hotel.

Redevelopment Proposals: 4.3.1 Midtown Center at ISB and MLK – Site 1

Town Square Option



View looking northwest; note clock tower and transit stop.



View south at town square.



Site plan with parking garages with solar panel shading devices.



View east at new townhomes and apartments. Magnolia St. is on the left and Foote St. at right.

Redevelopment Proposals: 4.4 Huger Park at MLK & Orange Ave. – Site 2

In this neighborhood center (NCMU District 2) at Orange Ave. and MLK Blvd., Huger Park is defined as a green public square with the addition of new two & three story, mixed-use and small commercial buildings (shown in white). New street trees, street lights, and distinctive crosswalks continue the midtown identity of the area.



Birdseye view looking southwest at Huger Park, at the intersection of Orange Ave. and MLK Blvd.



Orange Ave. & MLK intersection looking north west



Huger Park looking northeast. Existing buildings shown in peach, new buildings in white.

Redevelopment Proposals: 4.4.1 Huger Park at MLK & Orange Ave. – Site 2



Huger Park – looking northeast.



Orange Ave. & MLK intersection looking west



Three story mixed use with residential across from Huger Park



Redevelopment Proposals:

4.5 Redevelopment Site 3:

Northeast corner of ISB & Nova Rd.

This property is strategically located at the intersection of two major arterial streets. It is an excellent location for national lodging, restaurant and food service development. The drawing (shown at left) illustrates a potential site design for a hotel and restaurant with adequate parking, landscape and stormwater infrastructure. Both the hotel and restaurant address the street as required by the Midtown Development Standards District 3.

Site Redevelopment Summary

6.55 acres

Hotel & Meeting Rooms – 5 Floors, 180 Rooms

Restaurant – 1 Floor

145,000 s.f.

270 parking spaces

256 s.f. per space



Redevelopment Proposals:
4.6 Redevelopment Site
4a: ISB infill west

Site Redevelopment Summaries

North Site: 1.33 acres
 Building A – 1 floor, 2,700 s.f. office
 Building B – 2 floors, 11,000 s.f.
 mixed use

65 parking spaces
 211 s.f. per space

Southwest Site: 1.13 acres
 Building A – 2 floors, 14,200 s.f.
 mixed use

55 parking spaces
 258 s.f. per space

Southeast Site: 2.44 acres
 Building A – 2 floors, 21,600 s.f.
 Building B – 3 floors, 50,000 s.f.
 mixed use w/ 35 res. units

146 parking spaces
 160 s.f. per space retail & 1.5 spaces per
 res. unit



Redevelopment Proposals:

4.7 Redevelopment Site

4b: ISB infill central

Site Redevelopment

Summaries

Southwest Site: 3.75 acres

Building A – 2 floor garage townhomes

12 parking spaces

Building B – 2 flrs, 16,500 s.f. mixed use

Building C – 1 flr, 33,200 s.f. retail

167 parking spaces

297 s.f. per space

Southeast Site: 3.88 acres

Building A – 3 flrs, 100,400 s.f. mixed use w/

10,400 s.f. ground

flr. retail (@ corners)

& 100 res. units

207 parking spaces

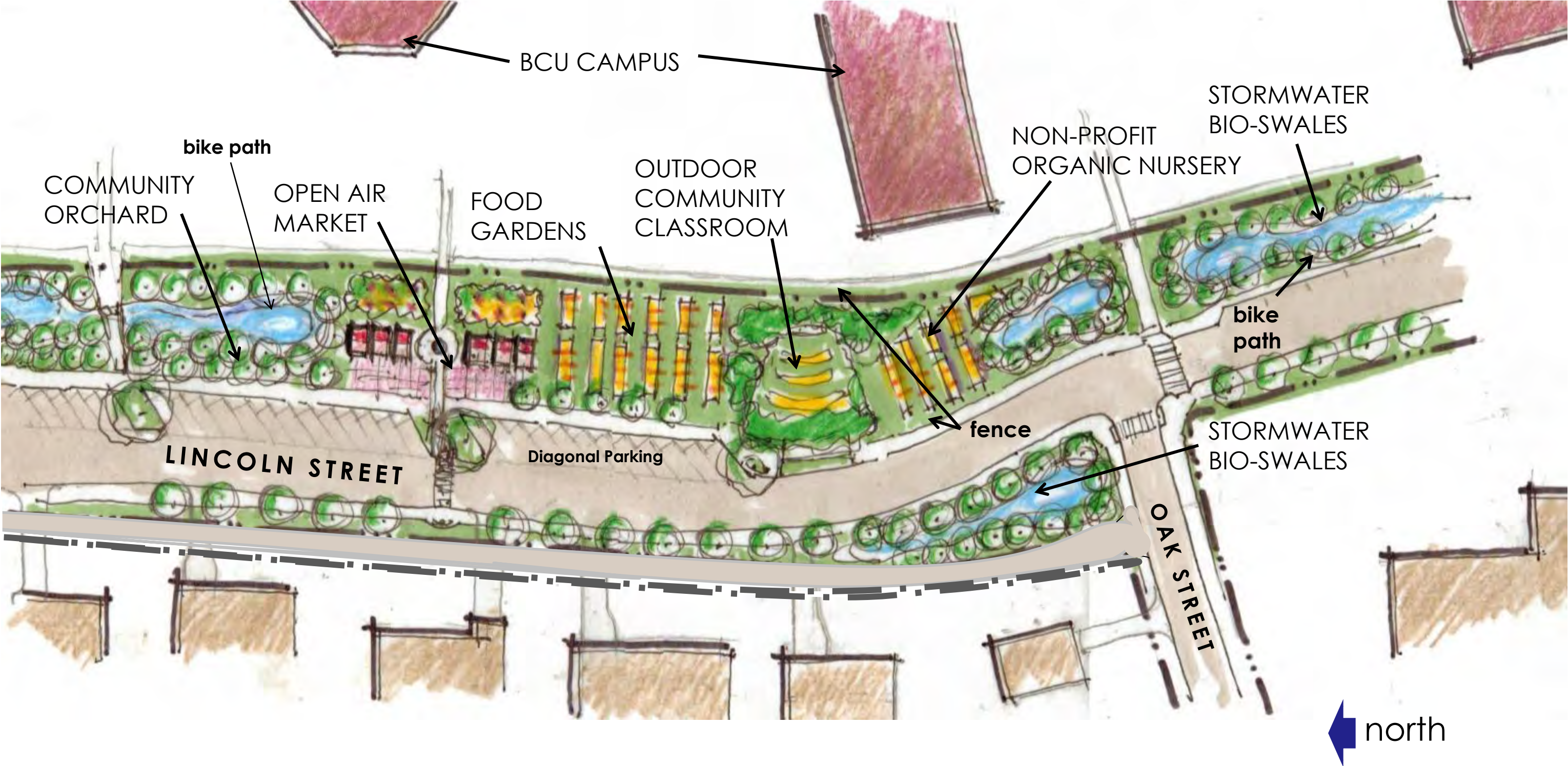
182 s.f. per space retail & 1.5

spaces per res. unit

Master Plan: 4.8 Lincoln Street Greenway – Site 7

Public Infrastructure Redevelopment Site

The Lincoln Street Greenway is an infrastructure improvement project in the wide 135" + - R.O.W of Lincoln Street located along the west edge of the BCU campus. The greenway will include a chain of public parks, community orchards, food gardens & markets, stormwater infrastructure and 248 public / campus parking spaces.



Master Plan: 4.9 Dickerson Center Park Expansion Plan – Site 9

A new public park that includes both active and passive recreational uses & facilities. The proposed location is west of the existing Dickerson Community Center on existing Housing Authority property. Land acquisition through “land swaps” between the city of Daytona Beach and the Housing Authority would be required for the project. This location offers a number of benefits to the community:

- It is centrally located in the portion of Midtown that is south of ISB.
- It would not remove any private land off of the tax rolls or reduce property tax revenue. Over time, it would increase tax revenue due to the development opportunities adjacent to the new park and green spaces.
- It would be connected with the existing Dickerson Center programs.
- The facility program would be developed by the community and would include the phased development of a football / soccer field, baseball / softball field, playgrounds, picnic areas + Dickerson Center renovation / expansion.
- It is near the neighborhood schools and would compliment the school facilities.
- The new park would present new development opportunities for new work force, market rate and affordable housing.

Legend

1. Dickerson Center
2. Community Hall
3. Pool, Spray Fountain & Dressing Rooms
4. BBQ & Picnic Area + Public Parking
5. Baseball & Softball
6. Football & Soccer
7. Playground
8. Social Green
9. New Market Rate, Workforce & Affordable Housing



Master Plan: 4.10 Thurman Gardens – Site 10

Historic Howard Thurman House & adjacent properties

The site is located in the southern part of the Midtown community at Whitehall Street. The railroad tracks are two blocks to the east and there is a public golf course located a few blocks south of the site. The historic home of Howard Thurman will be renovated and featured on the Black Heritage Trail. The surrounding area would be developed as an urban infill project, in keeping with LEED for Neighborhood Development, Smart Growth and New Urbanist recommendations. In addition to the Howard Thurman home there will be a family center, chapel, community garden, and an assisted living home.

- The majority of this block is currently owned by the city of Daytona Beach. Therefore, it would not remove any private land off of the tax rolls or reduce property tax revenue. Over time, it would increase tax revenue due to the development opportunities adjacent to the new park and green spaces
- This location is appropriate for an assisted living and senior housing development because it is in a relatively quiet part of town, yet near to the Bellevue and Huger Park Neighborhood Centers.
- If additional land acquisition is not possible at this location; other possible sites for senior housing are: the site located west of the intersection of Keech St. and Oak St. (where an existing “boarded up” housing complex is scheduled to be demolished) and future sites that would be adjacent to the proposed Dickerson Park Expansion (refer to page 42).

Legend

1. Renovated Thurman House
2. Family Center
3. Chapel
4. Community Garden
5. Assisted Living Center



5. Development Standards & Design Guidelines

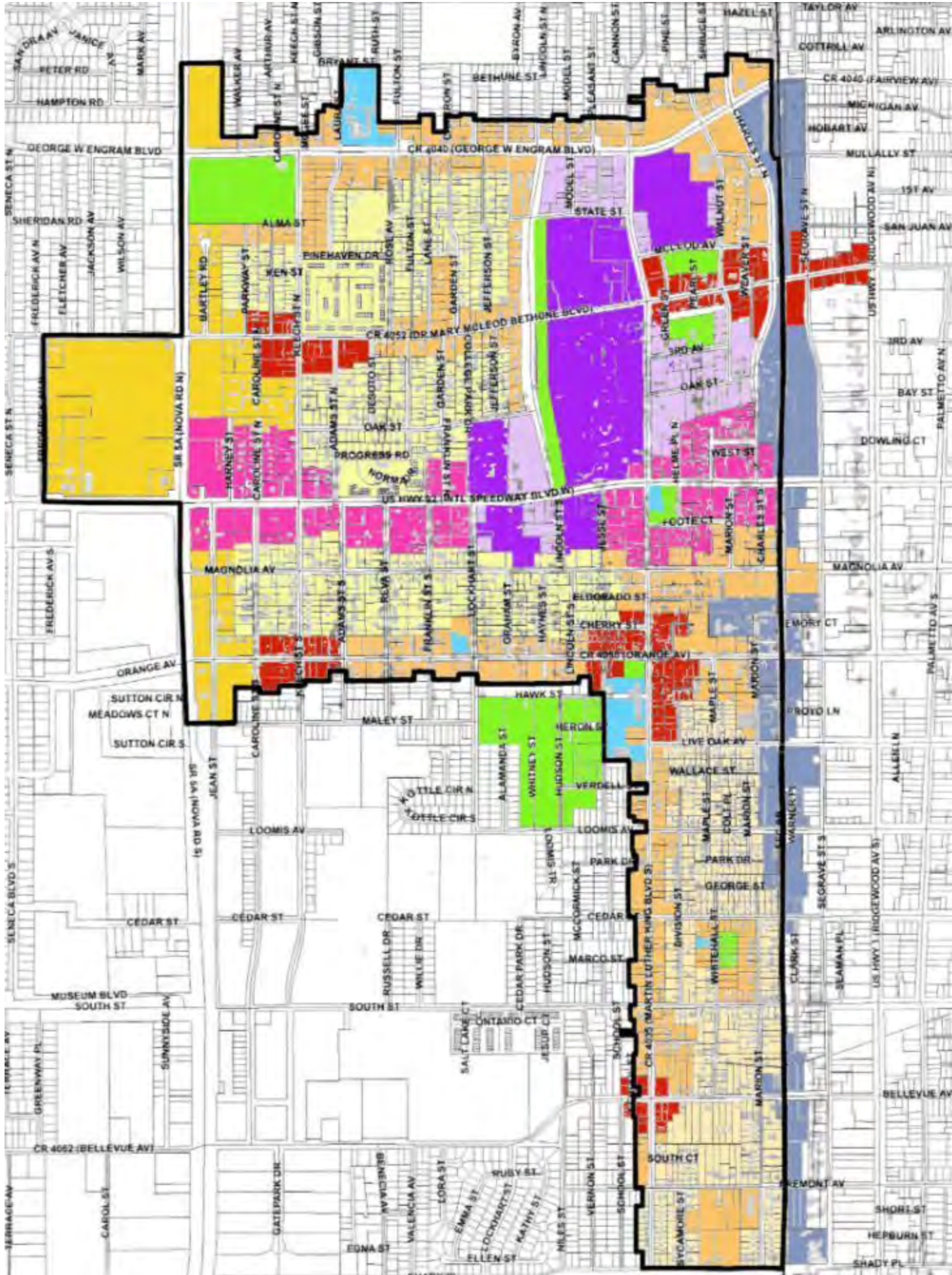
This document supplements the Land Development Regulations for the City of Daytona Beach - Midtown Redevelopment District. Refer to the LDR for a detailed list of permitted uses and environmental regulations in the district. These Development Standards & Design Guidelines are specific to the unique characteristics of the Midtown CRA area and they compliment the Land Development Regulations. All development in the Midtown District is subject to the requirements herein + all applicable requirements in the Land Development Regulations and the Comprehensive Plan.

5.1 MIDTOWN DEVELOPMENT DISTRICTS

Legend

- 1 – MIDTOWN CENTER MIXED-USE
- 2 – NEIGHBORHOOD CENTER MIXED-USE
- 3 – ACTIVITY CENTER MIXED-USE
- 4 – NEIGHBORHOOD TRANSITION
- 5 – RESIDENTIAL PRESERVATION 1
- 6 – RESIDENTIAL PRESERVATION HISTORIC INFILL
- 7 – ECO INDUSTRIAL
- 8 – UNIVERSITY TRANSITION

- – BETHUNE COOKMAN UNIVERSITY
- – PUBLIC PARKS AND GREENS
- – PUBLIC SPACES, BUILDINGS, AND MONUMENTS



[ZONE 1] MIDTOWN CENTER MIXED USE (MC-MU)

The Midtown Center Mixed Use (MC-MU) which focuses on the intersection of ISB & MLK and along ISB, as shown (left). The intersection of ISB & MLK is conceived as a vibrant urban center and the heart of the Midtown District. It includes a public square near the intersection and establishes new connections with Bethune Cookman University. It accommodates a mix of medium and large scale uses and public gathering spaces. Multi-family residential and residential units located above commercial uses are encouraged. Off street parking will be located behind or on the side of buildings. Commercial uses included retail, office, entertainment, institutional and services that provide goods and services for residents of the immediate neighborhoods and the Daytona Beach area. The Midtown Center Square will become an important destination as part of the Black Heritage Trail.

Architectural Standards:

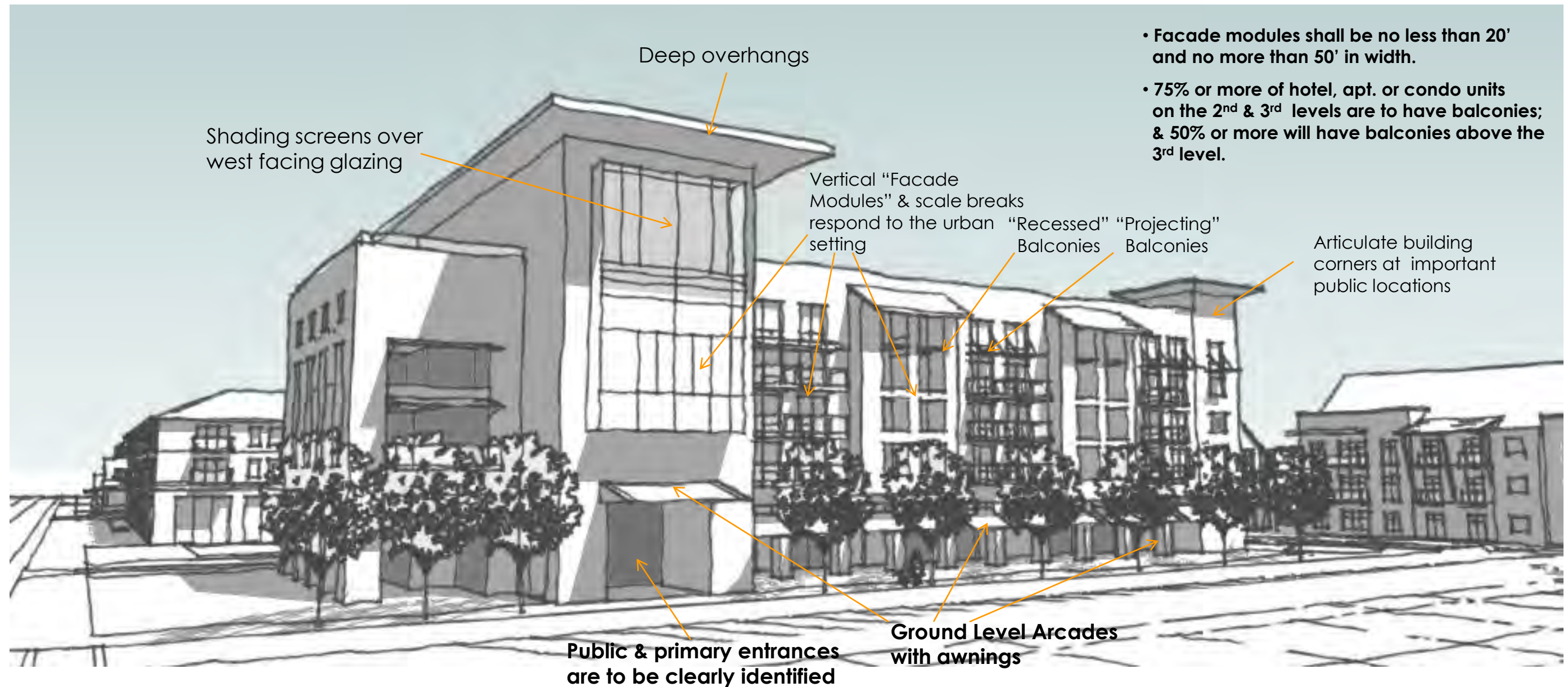
- 1.1 Facade modules shall have an emphasis on vertical proportions with dim. no less than 20' wide and no more than 50' in width.
 - 1.2 All glazing at the ground level should be clear glass or a type that allows a view into the interior spaces.
 - 1.3 Buildings with ground floor commercial uses shall have a 12' or higher ceiling.
 - 1.4 Arcades and colonnades shall have a 12' minimum deep clear dimension.
 - 1.5 Arcade and colonnade column bay proportions shall emphasize the vertical dimension. Vertical dim. will be equal to or more than the horiz. dim.
 - 1.6 Materials & design elements that are encouraged:
 - masonry & stucco exterior finishes
 - arcades & awnings
 - balconies, galleries & roof terraces
 - porches, breezeways, pergolas, sun shades
 - transom windows above doors
 - space for outdoor dining
 - 1.7 Prohibited materials & elements:
 - EIFS (exterior insulation finish systems) at the ground floor
 - mirror glass, vinyl siding, window AC units, drive thru facilities
- Both traditional & contemporary architecture are appropriate in the MC-MU district.**



The intended scale & character of Building Design in Zone 1 (MC-MU).

Development Standards & Design Guidelines: Design Guideline Illustration

The architectural character of Daytona Beach Midtown is based on a response to the climate and history of coastal central Florida and the Midtown District. Examples of appropriate architectural elements are: deeply recessed windows, canopies and generous overhangs that provide shade and protection from sun and rain; arcades and awnings on primary streets, squares and public spaces. Both neo – historical and contemporary variations of “Coastal Florida - Urban Architecture” are appropriate and encouraged in Midtown.



This illustration shows a hotel with retail at the ground floor located in Zone 1 MC-MU . It fronts on a public street / sidewalk, green or square. This building scale (height and massing) is permitted in Zones 1, 3 and 8. The architectural example is a contemporary urban building that addresses Midtown and coastal Florida design patterns and illustrates the intent of the Design Guidelines.

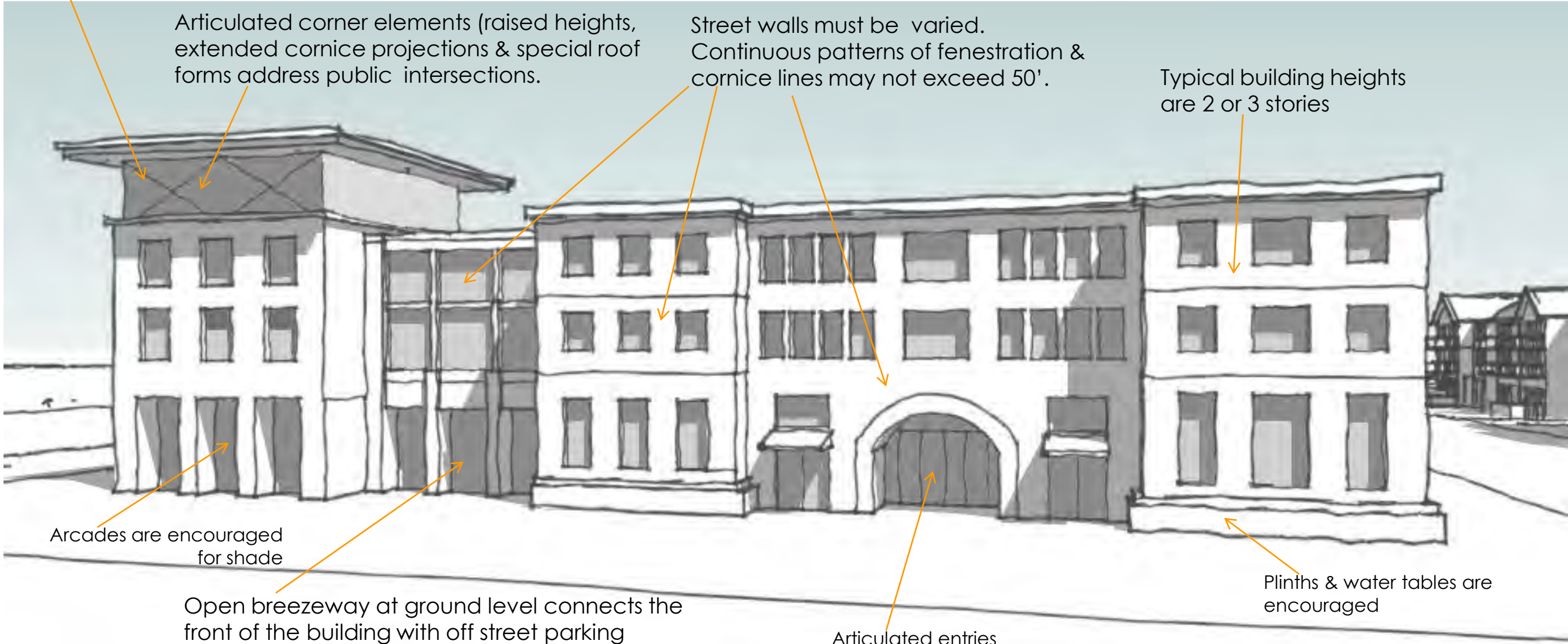
Development Standards & Design Guidelines: Design Guideline Illustration

Clerestories are encouraged for day lighting

Articulated corner elements (raised heights, extended cornice projections & special roof forms address public intersections.

Street walls must be varied. Continuous patterns of fenestration & cornice lines may not exceed 50'.

Typical building heights are 2 or 3 stories



Arcades are encouraged for shade

Open breezeway at ground level connects the front of the building with off street parking located behind the building.

Articulated entries are encouraged

Plinths & water tables are encouraged

This illustration shows a retail/office building; it is permitted in Zones 1,2,3,4,7 and 8. The 2nd and 3rd floors could also include residential uses.

Dimensional Standards

STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL	
DENSITY	DENSITY, MAX. (DU/AC)	80 / ACRE	50/ACRE	N/A
	LOT SIZE, MINIMUM	N/A	N/A	N/A
LOT AREA	LOT SIZE, MAXIMUM	N/A	N/A	N/A
	BUILDING COVERAGE MAX. (%)	75% * *(DOES NOT INCLUDE PARKING STRUCTURES)	60% * *(DOES NOT INCLUDE PARKING STRUCTURES)	N/A
	IMPERVIOUS COVERAGE	90%	85%	N/A
SETBACKS	FRONT	BUILDINGS: 15' MINIMUM; 25' MAXIMUM PAVING: 10'		
	SIDE	BUILDINGS: WHEN ABUTTING A STREET: 15 FT MINIMUM, 20 MAXIMUM IF PARCEL IS LESS THAN 100 FT. IN WIDTH: 10 FT. MINIMUM, 15' MAXIMUM NO SIDE YARD SHALL BE LESS THAN 10 FT. PAVING: 5'		
	REAR, MINIMUM	BUILDINGS: 15' PAVING: 10'		
	CORNER, MINIMUM	N/A		
	ACCESSORY STRUCTURE	10' IF 600 S.F. IN SIZE OR SMALLER; OTHERWISE SAME AS PRINCIPAL USE		
	PARKING GARAGE SETBACK, MINIMUM	60' (LEAVES ADEQUATE SPACE FOR LINER BUILDINGS AND OTHER ACTIVE USES THAT ADDRESS THE STREET) (ON THE PRIMARY STREET FRONTAGE) SECONDARY STREETS – SAME AS BUILDINGS (SHOWN ABOVE)		
	HEIGHT, MAXIMUM STORIES	2 STORY MINIMUM (EXCEPT FOR LARGE ASSEMBLY USES) 5 STOREY MAXIMUM		
MASSING	ACCESSORY STRUCTURES	2.5 STORIES MAXIMUM HEIGHT		

Dimensional Standards

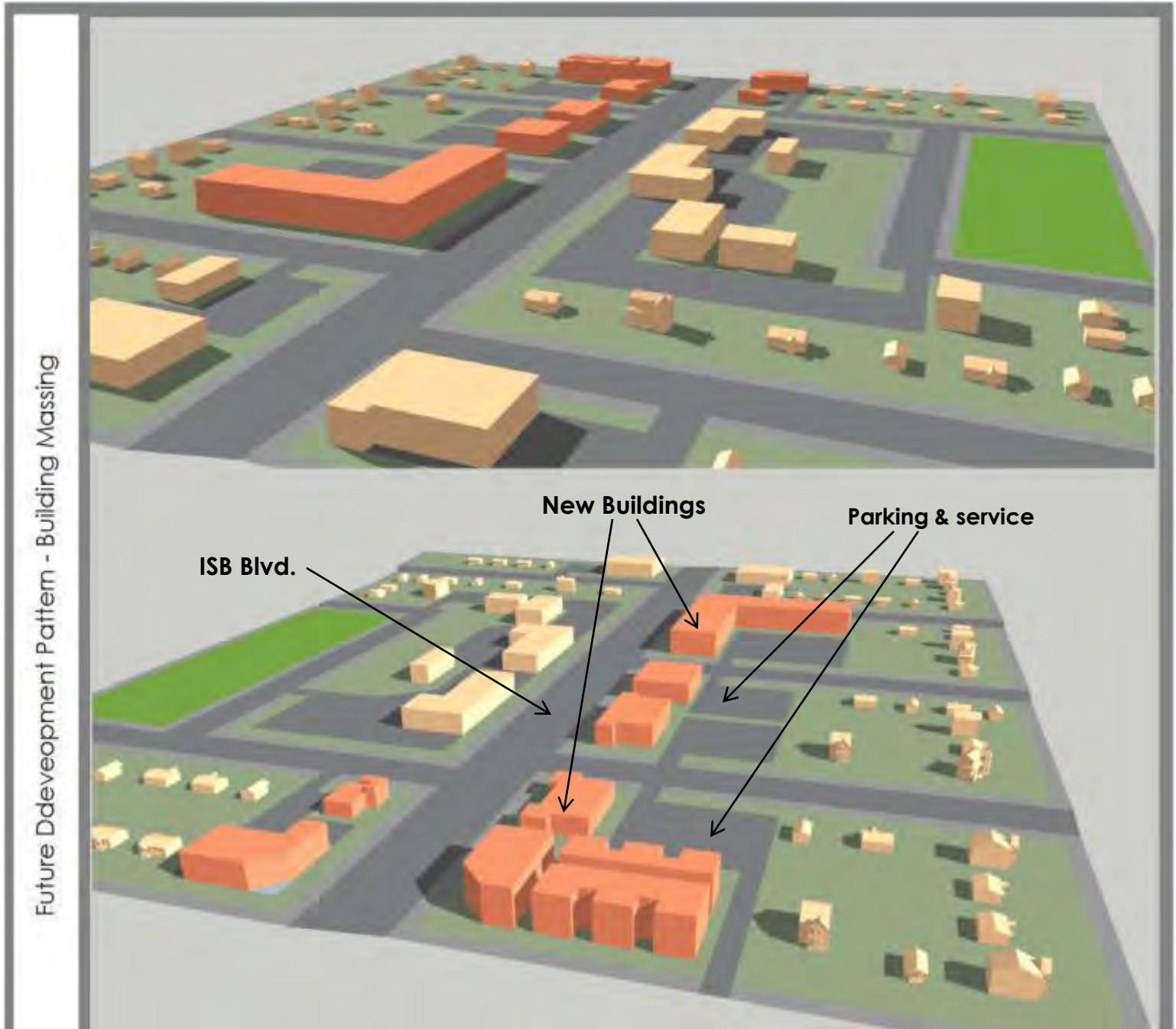
STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
PARKING	<ul style="list-style-type: none"> RESTAURANTS AND CAFES: 1 SPACE/100 S.F. AREA GENERAL RETAIL: 1 SPACE/250 S.F. AREA OFFICE: 1 SPACE/400 S.F. AREA ON STREET PUBLIC PARKING MAY BE COUNTED WITHIN A 1000' RADIUS PARKING SHARING MAY BE USED FOR COMPATIBLE USES THAT HAVE DIFFERENT PARKING SCHEDULES 	<ul style="list-style-type: none"> 1 SPACE/ STUDIO, AND 1 OR 2 BEDROOM UNITS 2 SPACES/3 BEDROOM UNITS 3 SPACES/4 BEDROOM UNITS 1 SPACE PER SLEEPING UNIT/TRANSIENT HOUSING ON STREET PARKING MAY BE COUNTED WITHIN A 1000' RADIUS 	N/A
	ALL OFFSTREET PARKING IS TO BE LOCATED AT THE MIDDLE, REAR & SIDE OF THE LOT. IT IS TO BE PLACED BEHIND AND TO THE SIDE OF EXISTING AND NEW BUILDINGS. PARKING LOTS ARE NOT ALLOWED IN FRONT OF BUILDINGS. PARKING THAT ABUTS THE FRONT AND SIDE STREET PROPERTY LINES ARE TO BE SCREENED BY LOW OPAQUE WALLS OR HEDGES. WALLS AND HEDGES ARE TO BE BETWEEN 2' AND 4' HIGH.		PARKING THAT IS LOCATED WITHIN 30' OF A REAR OR SIDE PROP. LINE OF EXISTING RES. USES MUST HAVE AN OPAQUE FENCE OR HEDGE BETWEEN 4'-8" & 6' HIGH TO VISUALLY SCREEN THE PARKING AREA.
LANDSCAPE	<ul style="list-style-type: none"> ONE STREET TREE PER 25' OF STREET FRONTAGE TREE SIZE AND SPECIES ARE TO BE SELECTED FROM THE CITY OF DAYTONA BEACH APPROVED STREET TREE LIST MAJOR STREETS (ISB, MLX, ORANGE, MMBB, KEECH & BELLVUE): STREET TREES WILL BE PROVIDED AS PART OF THE PUBLIC SPACE INFRASTRUCTURE SECONDARY AND MINOR STREETS: STREET TREES WILL BE PROVIDED BY THE OWNER/DEVELOPER 		
ARCHITECTURE	STREET WALL	FIRST FLOOR, RETAIL, OFFICE AND PUBLIC USE MUST BE 60% OR MORE TRANSPARENT. THIS STANDARD WILL BE ACHIEVED FOR "BIG BOX" STORES, MOVIE THEATERS AND OTHER LARGE OPAQUE SURFACES BY WRAPPING THE SOLID SURFACES WITH LINER BUILDINGS THAT MEET THE 60% TRANSPARENCY STANDARD.	
	BALCONIES	BUILDING FENESTRATION PATTERN AND CORNICE LINE MAY NOT BE CONTINUOUS FOR MORE THAN 50'	
DETAILS	<ul style="list-style-type: none"> 75% OR MORE OF RESIDENTIAL UNITS ON THE 2ND FLOOR MUST HAVE COVERED BALCONIES 50% OR MORE OF THE RESIDENTIAL UNITS ON THE 3RD FLOOR MUST HAVE COVERED BALCONIES BALCONIES MUST BE 6' DEEP OR MORE 		
	<ul style="list-style-type: none"> BUILDING DORMERS & CUPOLAS WITH GLAZING MUST BE IN AN OCCUPIED ROOM OR SERVE AS LIGHT WELLS COVERED ARCADES ARE ENCOURAGED ON THE GROUND FLOOR 	<ul style="list-style-type: none"> PRIVATE PORCHES AND ENTRIES ARE TO BE RAISED 2' TO 3' ABOVE GRADE PORCHES MUST BE 8' DEEP OR MORE 	

[Zone 1] MIDTOWN CENTER MIXED-USE



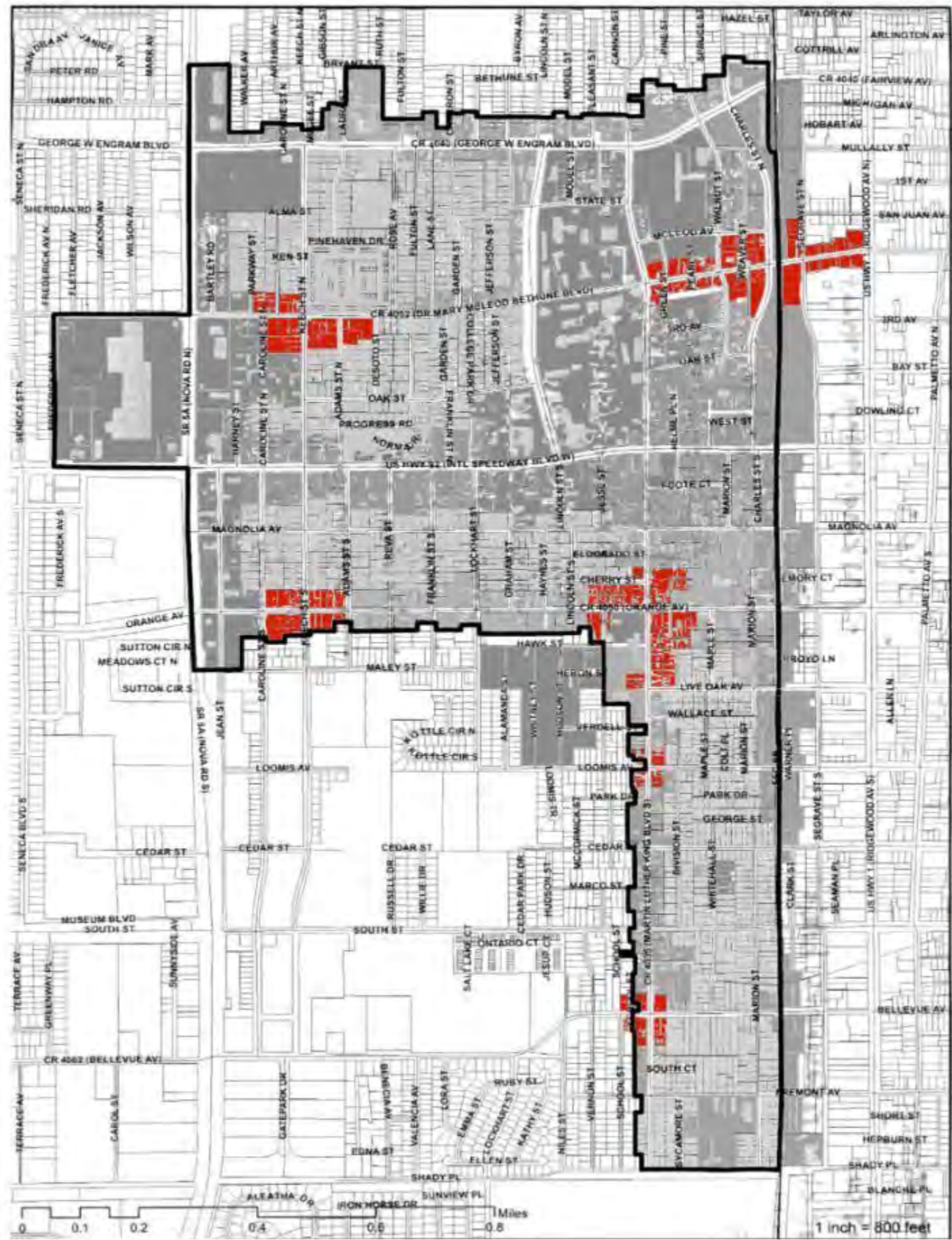
Future Ddevelopment Pattern

Additional Notes



Future Ddevelopment Pattern - Building Massing

[ZONE 2] NEIGHBORHOOD CENTER MIXED USE (NC-MU)



The Neighborhood Center Mixed-Use (NC-MU) is established to accommodate a mix of residential and small-scale, low-intensity, and “convenience” retail and services that provide goods/services to residents of the immediate neighborhood (i.e. personal services, restaurants, and limited retail.)

Development in the district should not include uses of a size that is out of scale with a residential neighborhood. Residential uses are encouraged on the upper floors of nonresidential establishments. The district is subject to standards intended to ensure development is consistent with the neighborhood scale and compatible with the surrounding uses.

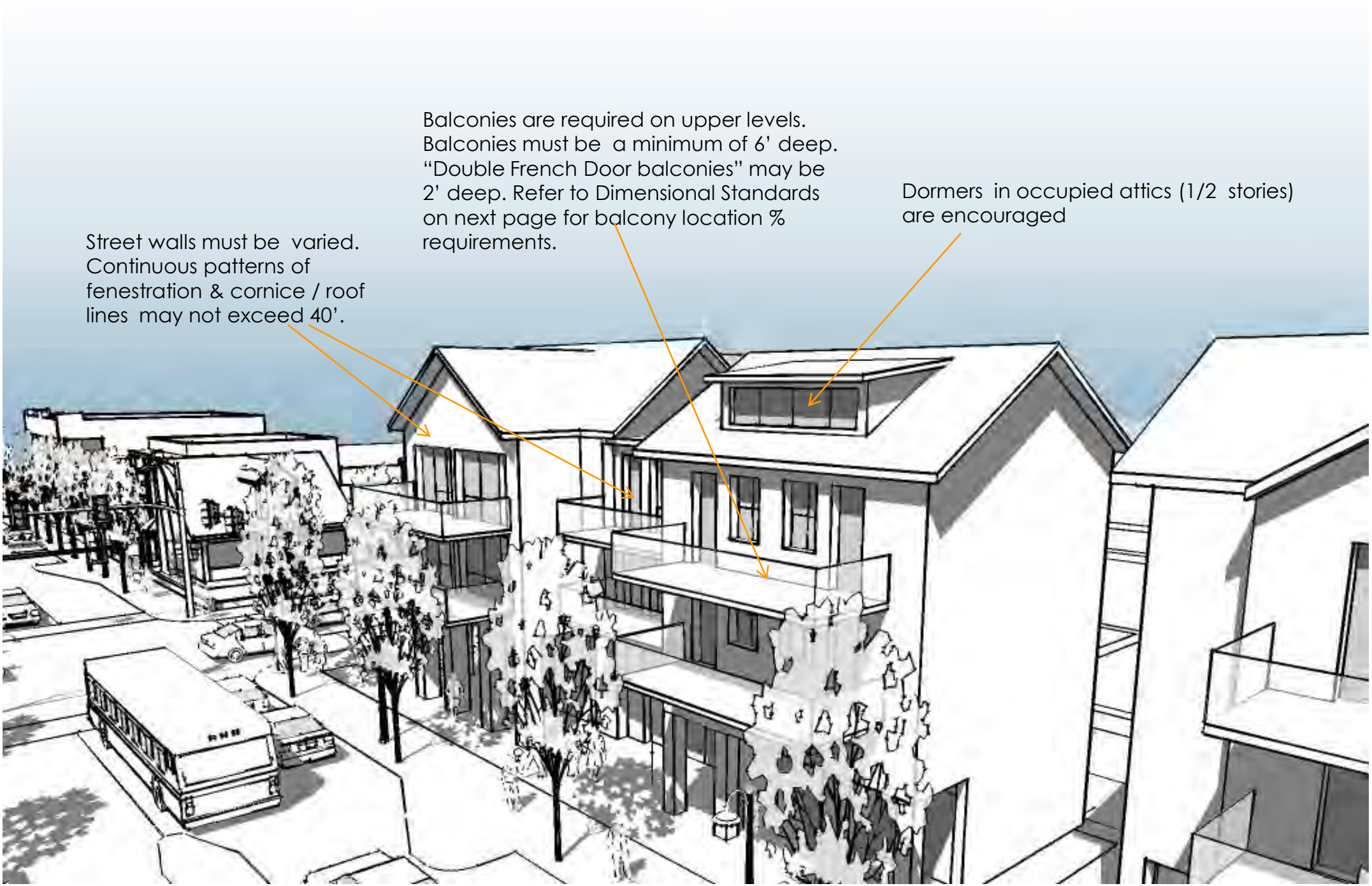
Architectural Standards:

- 2.1 Facade modules shall have an emphasis on vertical proportions with dim. no less than 20' wide and no more than 40' in width.
- 2.2 All glazing at the ground level should be clear glass or a type that allows a view into the interior spaces.
- 2.3 Buildings with ground floor commercial uses shall have a 12' or higher ceiling.
- 2.4 Arcades and colonnades shall have a 10' minimum deep clear dimension.
- 2.5 Arcade and colonnade column bay proportions shall emphasize the vertical dimension. Vertical dim. will be equal to or more than the horiz. dim.
- 2.6 Materials & design elements that are encouraged:
 - masonry , stucco, cementitious siding & metal ext. finishes
 - arcades & awnings
 - balconies, galleries & roof terraces
 - porches, breezeways, pergolas, sun shades
 - transom windows above doors
 - space for outdoor dining
- 2.7 Prohibited materials & elements:
 - EIFS (exterior insulation finish systems) at the ground floor
 - mirror glass, vinyl siding, window AC units, drive thru facilities



The intended scale & character of Building Design in Zone 2 (NC-MU).

Development Standards & Design Guidelines: Design Guideline Illustration



This illustration shows a series of renovated existing commercial buildings in the background and new mixed use residential buildings (foreground). Adding 2 & 3 story buildings with residential uses on the upper floors (with parking in the rear) creates a pedestrian oriented "Main Street" along MLK Blvd. This building scale (height and massing) is permitted in Zones 1,2, 3, 4 and 8.

[ZONE 2] NEIGHBORHOOD CENTER MIXED USE (NC-MU)

Dimensional Standards

	STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
DENSITY	DENSITY, MAX (DU/AC)	40 / ACRE	20/ACRE	N/A
	LOT SIZE, MINIMUM	N/A	N/A	
LOT AREA	LOT SIZE, MINIMUM	N/A	N/A	THE LESSER OF 50 FEET OR THE AVERAGE LOT WIDTH OF LOTS WITHIN 300 FEET ALONG THE SAME BLOCK FACE, PROVIDED THERE ARE 7 OR MORE LOTS WITHIN THE 300 FOOT AREA.
	BUILDING COVERAGE MAX. (%)	70%	70%	60%
	IMPERVIOUS COVERAGE	90%	85%	70%
	FRONT	10' MINIMUM; 20' MAXIMUM		
SETBACKS	SIDE, MINIMUM	<ul style="list-style-type: none"> WHEN ABUTTING A STREET: 20 FT IF PARCEL IS LESS THAN 100 FT. IN WIDTH: 10 FT. NO SIDE YARD SHALL BE LESS THAN 5 FT. UNLESS IT IS PART OF A ZERO SIDE LOT LINE DEVELOPMENT 		<ul style="list-style-type: none"> WHEN ABUTTING A STREET: 5 FT NO SIDE YARD SHALL BE LESS THAN 5 FT. UNLESS IT IS PART OF A ZERO SIDE LOT LINE DEVELOPMENT
	REAR, MINIMUM	15'		10'
	CORNER, MINIMUM	15'		
	ACCESSORY STRUCTURE	5' IF 600 S.F. IN SIZE OR SMALLER; OTHERWISE SAME AS PRINCIPAL USE		
	GARAGE SETBACK, MINIMUM	20'		20' BEHIND FRONT FACADE, OTHERWISE SAME AS PRINCIPAL USE
MASSING	HEIGHT, MAXIMUM STORIES	3.5 STORIES		2.5 STORIES
	ACCESSORY STRUCTURE	N/A		33% OF PRINCIPAL USE

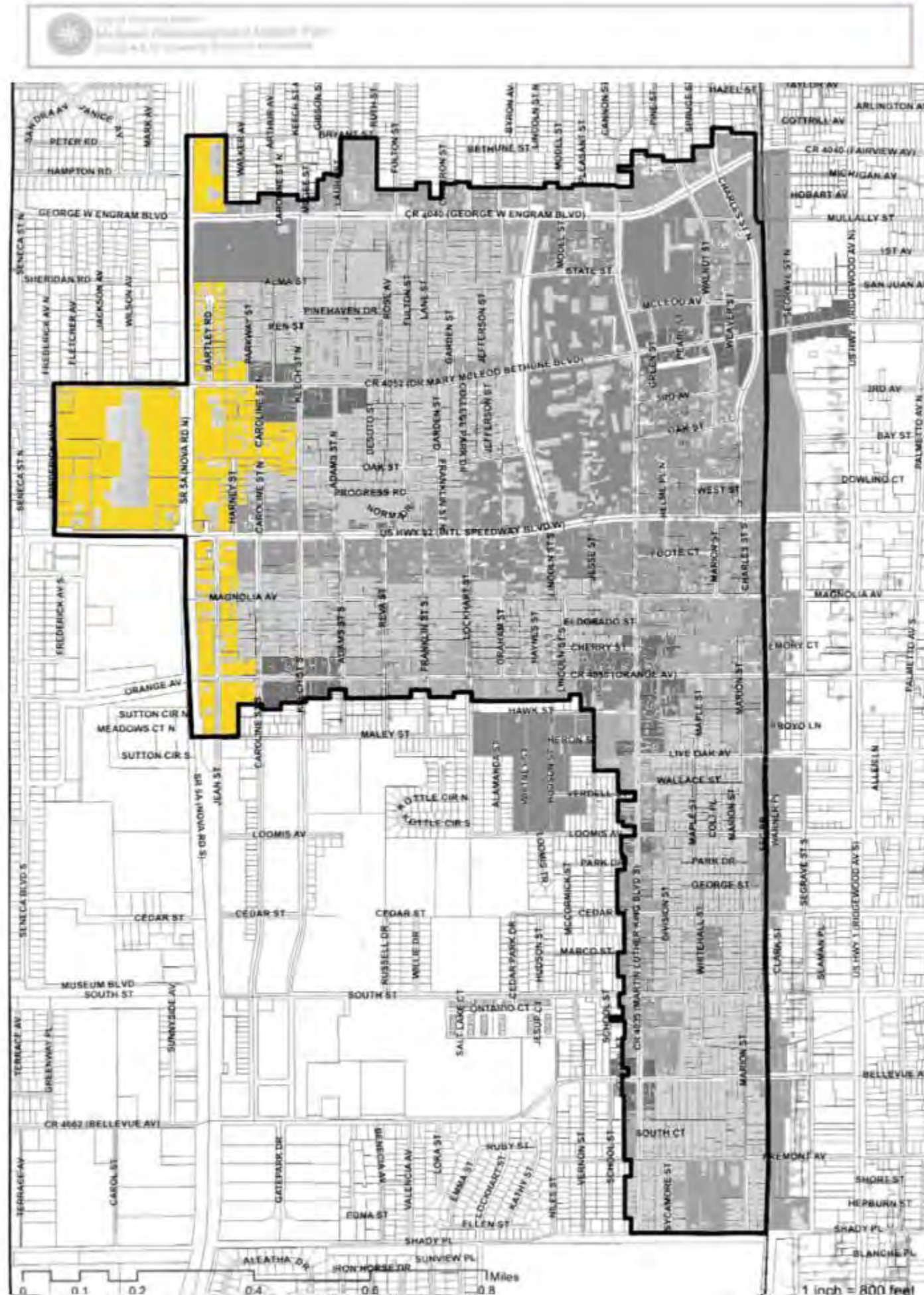
Dimensional Standards

	STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
PARKING		<ul style="list-style-type: none"> RESTAURANTS AND CAFES: 1 SPACE/100 S.F. AREA GENERAL RETAIL: 1 SPACE/250 S.F. AREA OFFICE: 1 SPACE/400 S.F. AREA ONSTREET PUBLIC PARKING MAY BE COUNTED WITHIN A 1000' RADIUS PARKING SHARING MAY BE USED FOR COMPATIBLE USES THAT HAVE DIFFERENT PARKING SCHEDULES 	<ul style="list-style-type: none"> 1 SPACE/ STUDIO, AND 1 OR 2 BEDROOM UNITS 2 SPACES/3 BEDROOM UNITS 3 SPACES/4 BEDROOM UNITS 1 SPACE PER SLEEPING UNIT/TRANSIENT HOUSING ON STREET PARKING MAY BE COUNTED WITHIN A 350' RADIUS 	<ul style="list-style-type: none"> 4 SPACES - MAXIMUM IN GARAGES CARPORTS AND DRIVES
		ALL OFFSTREET PARKING IS TO BE LOCATED AT THE MIDDLE, REAR & SIDE OF THE LOT. IT IS TO BE PLACED BEHIND AND TO THE SIDE OF EXISTING AND NEW BUILDINGS. PARKING LOTS ARE NOT ALLOWED IN FRONT OF BUILDINGS. PARKING THAT ABUTS THE FRONT AND SIDE STREET PROPERTY LINES ARE TO BE SCREENED BY LOW OPAQUE WALLS OR HEDGES. WALLS AND HEDGES ARE TO BE BETWEEN 2' AND 4' HIGH.		PARKING THAT IS LOCATED WITHIN 30' OF A REAR OR SIDE PROP. LINE NEXT TO EXISTING RESIDENTIAL USES MUST HAVE AN OPAQUE FENCE OR HEDGE BETWEEN 4'-8" & 6' HIGH TO VISUALLY SCREEN THE PARKING AREA.
LANDSCAPE	<ul style="list-style-type: none"> ONE STREET TREE PER 25' OF STREET FRONTAGE TREE SIZE AND SPECIES ARE TO BE SELECTED FROM THE CITY OF DAYTONA BEACH APPROVED STREET TREE LIST MAJOR STREETS: STREET TREES WILL BE PROVIDED AS PART OF THE PUBLIC SPACE INFRASTRUCTURE SECONDARY AND MINOR STREETS: STREET TREES WILL BE PROVIDED BY THE OWNER/DEVELOPER 			
ARCHITECTURE	STREET WALL	FIRST FLOOR, RETAIL, OFFICE AND PUBLIC USE MUST BE 60% OR MORE TRANSPARENT		
		BUILDING FENESTRATION PATTERN AND CORNICE LINE MAY NOT BE CONTINUOUS FOR MORE THAN 40'		
BALCONIES	<ul style="list-style-type: none"> 75% OR MORE OF RESIDENTIAL UNITS ON THE 2ND FLOOR MUST HAVE COVERED BALCONIES 50% OR MORE OF THE RESIDENTIAL UNITS ON THE 3RD FLOOR MUST HAVE COVERED BALCONIES BALCONIES MUST BE 6' DEEP OR MORE 			
DETAILS	<ul style="list-style-type: none"> BUILDING DORMERS WITH GLAZING MUST BE IN AN OCCUPIED ROOM OR SERVE AS LIGHT WELLS COVERED ARCADES ARE ENCOURAGED ON THE GROUND FLOOR 	<ul style="list-style-type: none"> PRIVATE PORCHES AND ENTRIES ARE TO BE RAISED 2' TO 3' ABOVE GRADE PORCHES MUST BE 8' DEEP OR MORE 		

[ZONE 2] NEIGHBORHOOD CENTER MIXED USE (NC-MU)



[ZONE 3] ACTIVITY CENTER MIXED-USE (AC-MU)



The Activity Center Mixed-Use (AC-MU) is established to accommodate a mix of medium and large scale uses and public gathering spaces. Multi-family residential and residential units located above smaller commercial uses are encouraged. This area includes large parcels that will accommodate large footprint retail and activity centers with sufficient off street parking. Buildings will address public streets and sidewalks with off street parking located at the rear and side of structures. Commercial uses include: retail, office, hospitality and service uses that provide goods/services to residents of the immediate neighborhood and the larger Daytona Beach area.

Architectural Standards:

- 3.1 Facade modules shall have an emphasis on vertical proportions with dim. no less than 20' wide and no more than 75' in width.
- 3.2 All glazing at the ground level should be clear glass or a type that allows a view into the interior spaces.
- 3.3 Buildings with ground floor commercial uses shall have a 12' or higher ceiling.
- 3.4 Arcades and colonnades shall have a 12' minimum deep clear dimension.

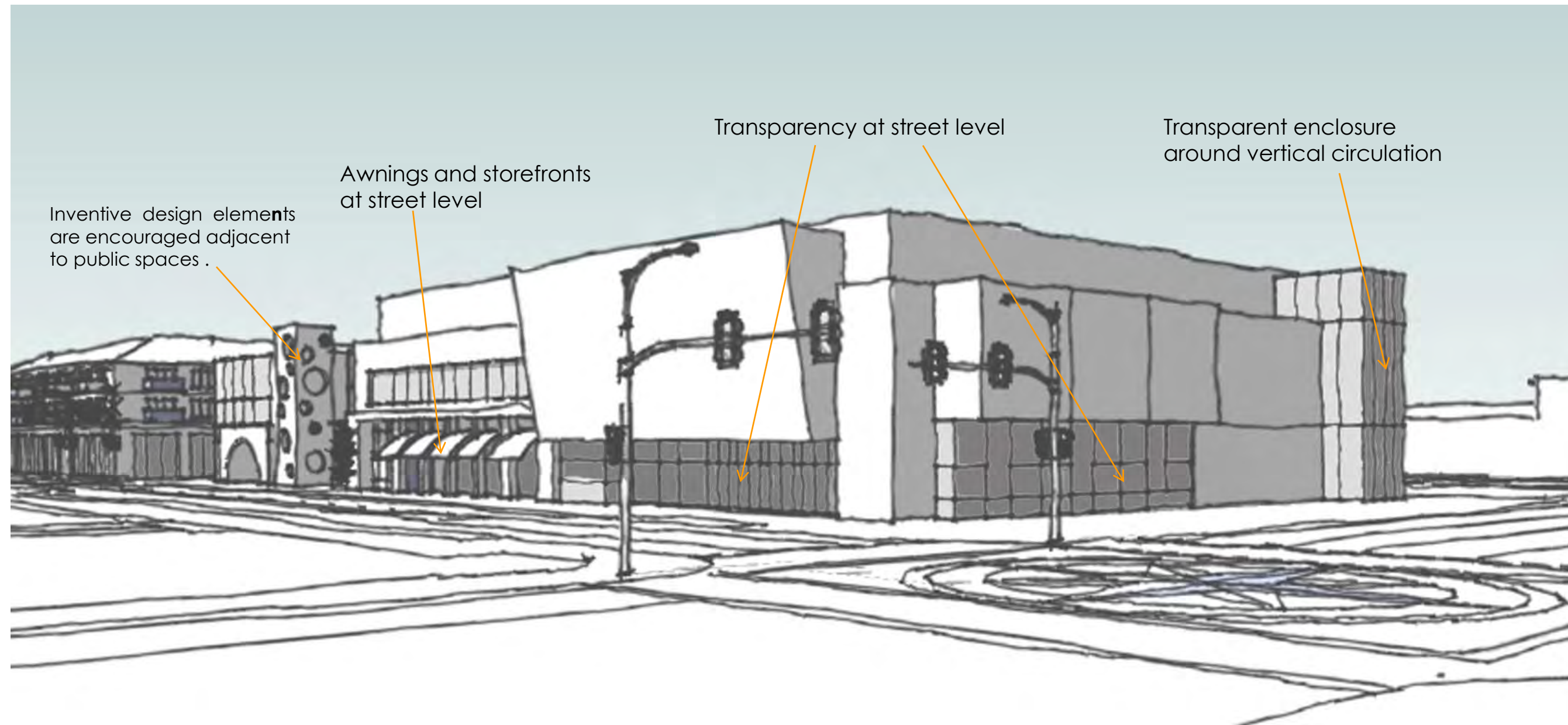
- 3.5 Arcade and colonnade column bay proportions shall emphasize the vertical dimension. Vertical dim. will be equal to or more than the horiz. dim.
- 3.6 Materials & design elements that are encouraged:
 - masonry, stucco & metal panel exterior finishes
 - arcades & awnings
 - balconies, galleries & roof terraces
 - breezeways, pergolas, sun shades, scrims & green walls
 - varied storefront patterns with distinctive entries & signage
 - covered areas for outdoor dining at restaurants
- 3.7 Prohibited materials & elements:
 - mirror glass, vinyl siding, window AC units

Both traditional & contemporary architecture are appropriate in the AC-MU district.



The intended scale & character of Building Design in Zone 3 (AC-MU).

Development Standards & Design Guidelines: Design Guideline Illustration



This illustration shows a movie theater wrapped with a liner building that includes transparency at the pedestrian scale. Large “Big Box” buildings with featureless blank facades must be wrapped on at least two (2) sides with pedestrian scaled elements that address the street and sidewalk. This building scale (height and massing) is permitted in Zones 1 and 3.

[ZONE 3] ACTIVITY CENTER MIXED USE (AC-MU)

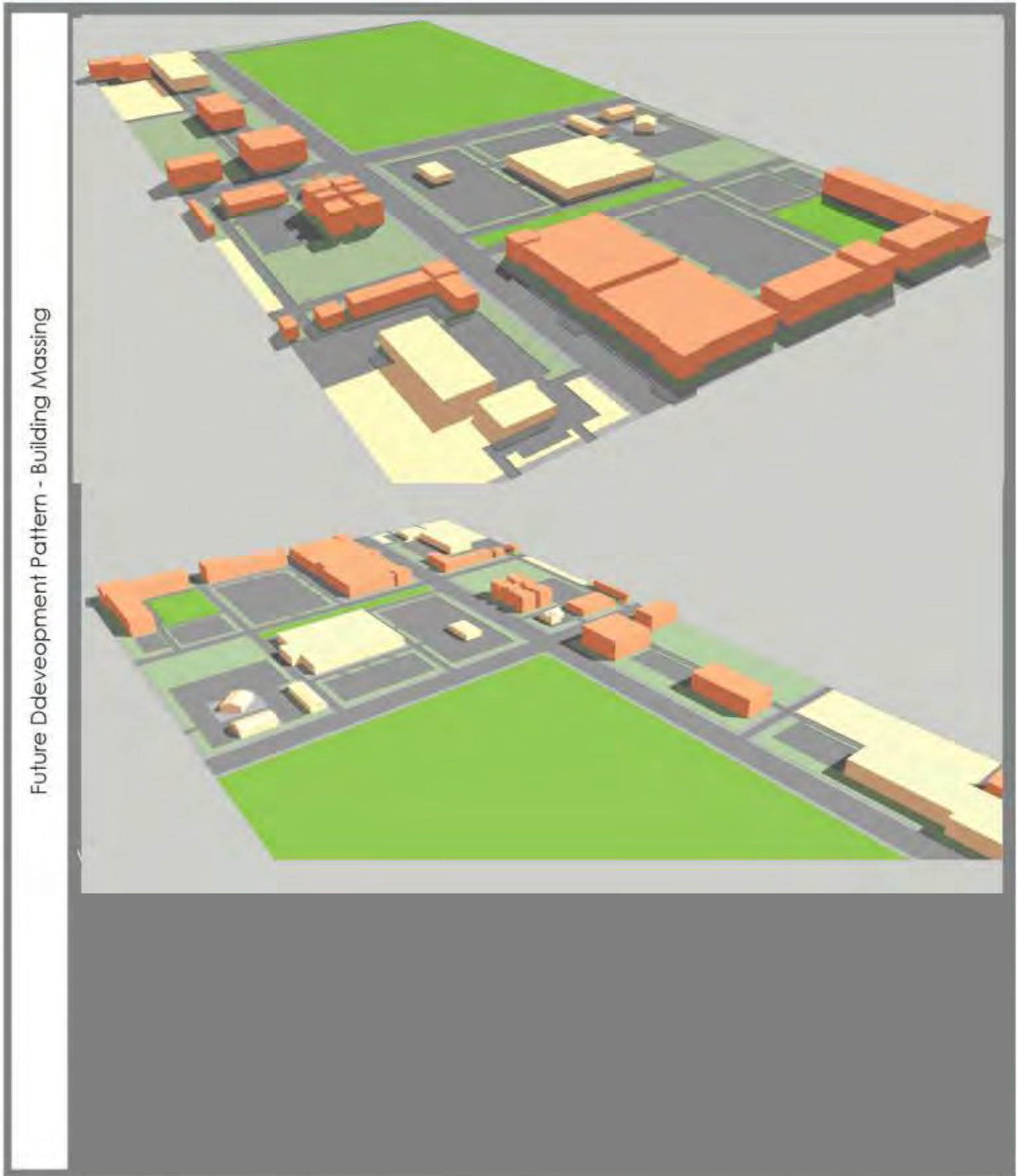
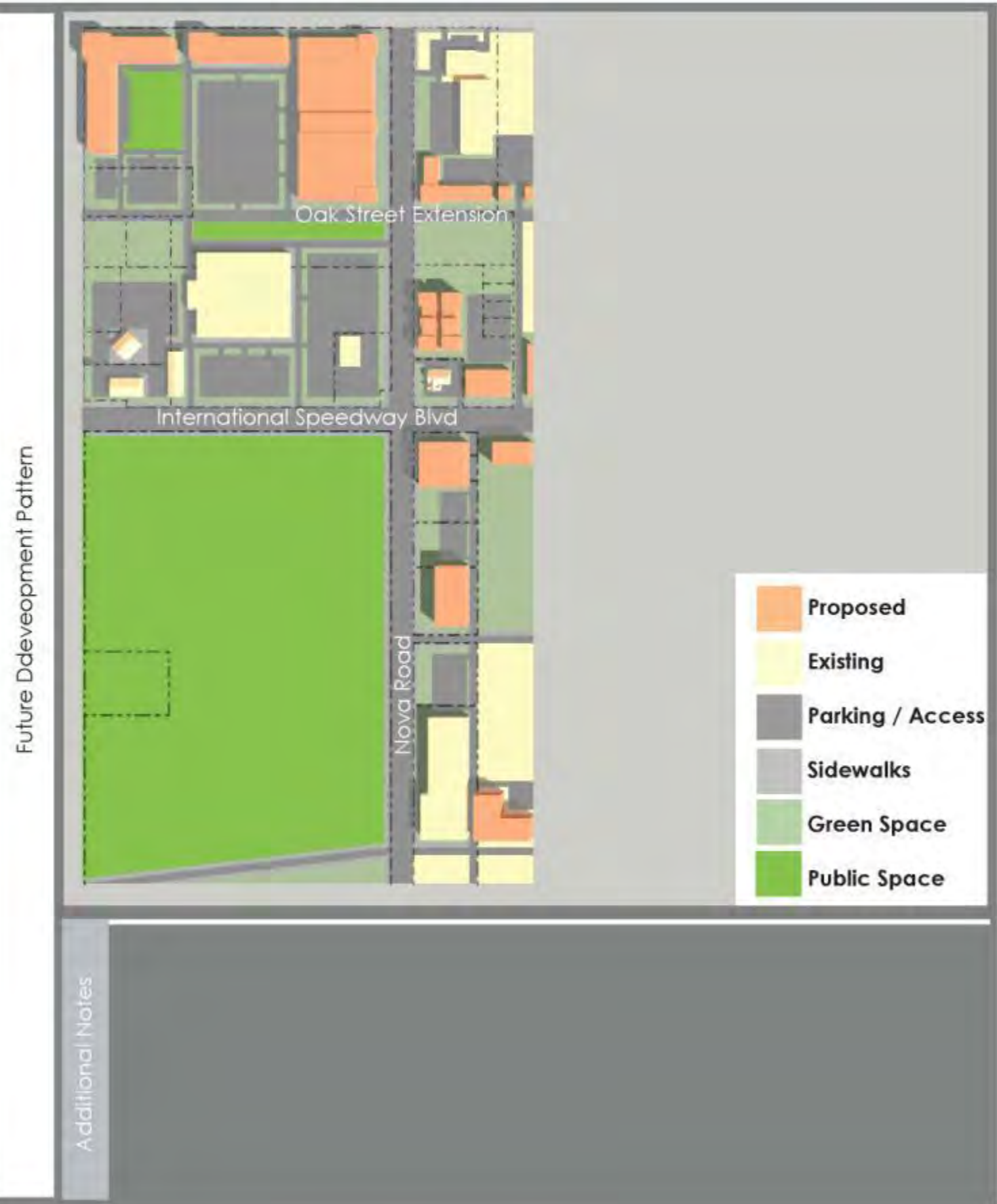
Dimensional Standards

	STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
DENSITY	DENSITY, MAX. (DU/AC)	N/A	50/ACRE	N/A
	LOT SIZE, MINIMUM	N/A	N/A	N/A
LOT AREA	LOT SIZE, MINIMUM	N/A	N/A	N/A
	BUILDING COVERAGE MAX. (%)	60%	60%	N/A
	IMPERVIOUS COVERAGE	85%	80%	N/A
SETBACKS	FRONT	BUILDINGS: 15' MINIMUM; 30' MAXIMUM PAVING: 15'		
	SIDE	BUILDINGS: WHEN ABUTTING A STREET: 20 FT IF PARCEL IS LESS THAN 100 FT. IN WIDTH: 15 FT. NO SIDE YARD SHALL BE LESS THAN 10 FT. PAVING: 5'		
	REAR, MINIMUM	BUILDINGS: 15' PAVING: 10'		
	CORNER, MINIMUM	N/A		
	ACCESSORY STRUCTURE	10' IF 600 S.F. IN SIZE OR SMALLER; OTHERWISE SAME AS PRINCIPAL USE		
	PARKING GARAGE SETBACK, MINIMUM	70' (LEAVES ADEQUATE SPACE FOR LINER BUILDINGS AND OTHER ACTIVE USES THAT ADDRESS THE STREET)		
	HEIGHT, MAXIMUM STORIES	5 STORIES		
MASSING	ACCESSORY STRUCTURE	N/A		

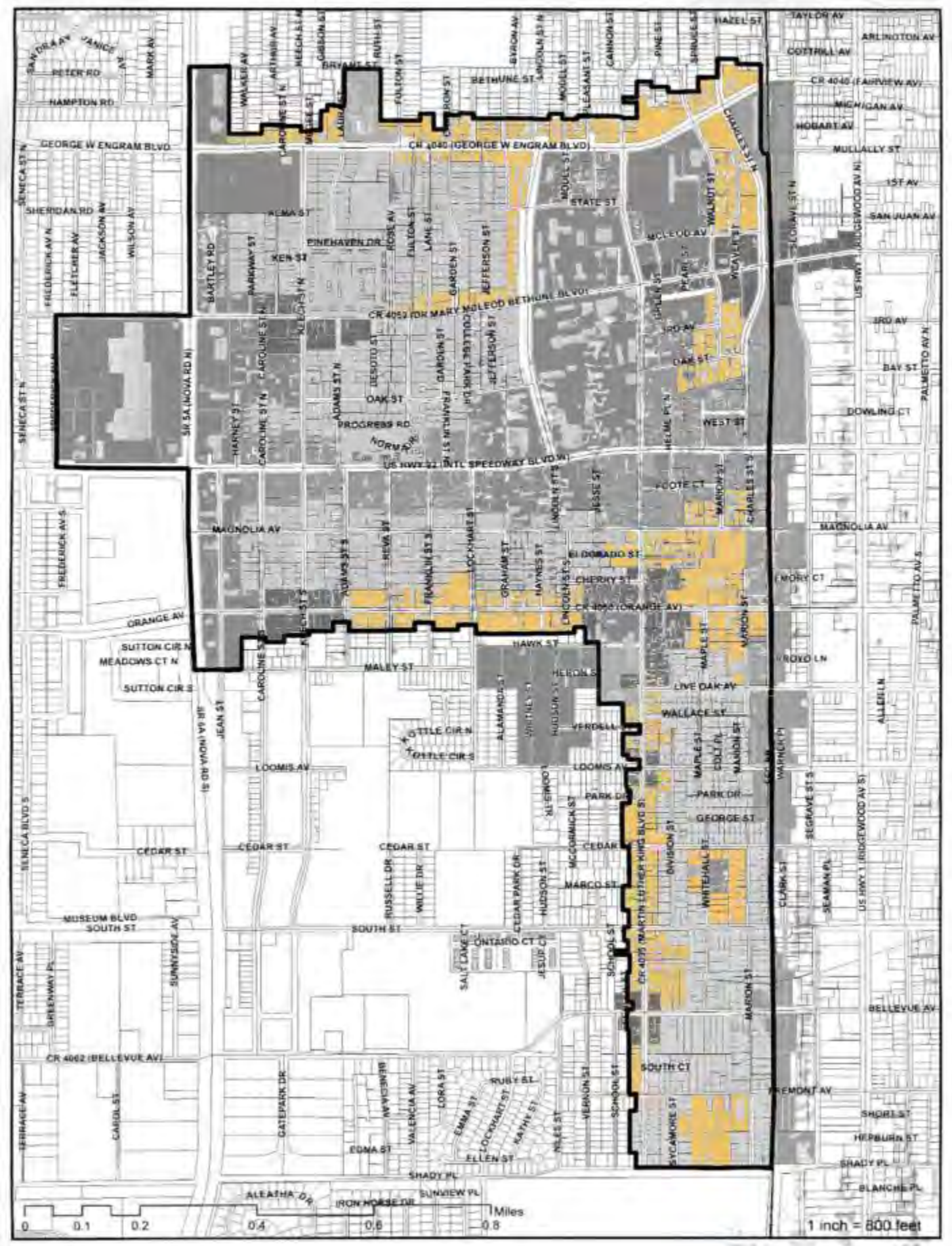
Dimensional Standards

	STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
PARKING		<ul style="list-style-type: none"> RESTAURANTS AND CAFES: 1 SPACE/100 S.F. AREA GENERAL RETAIL: 1 SPACE/250 S.F. AREA OFFICE: 1 SPACE/400 S.F. AREA ON STREET PUBLIC PARKING MAY BE COUNTED WITHIN A 1000' RADIUS PARKING SHARING MAY BE USED FOR COMPATIBLE USES THAT HAVE DIFFERENT PARKING SCHEDULES 	<ul style="list-style-type: none"> 1 SPACE/STUDIO, AND 1 OR 2 BEDROOM UNITS 2 SPACES/3 BEDROOM UNITS 3 SPACES/4 BEDROOM UNITS 1 SPACE PER SLEEPING UNIT/TRANSIENT HOUSING ON STREET PARKING MAY BE COUNTED WITHIN A 500' RADIUS 	N/A
		ALL OFFSTREET PARKING IS TO BE LOCATED AT THE MIDDLE, REAR & SIDE OF THE LOT. IT IS TO BE PLACED BEHIND AND TO THE SIDE OF EXISTING AND NEW BUILDINGS. PARKING LOTS ARE NOT ALLOWED IN FRONT OF BUILDINGS. PARKING THAT ABUTS THE FRONT AND SIDE STREET PROPERTY LINES ARE TO BE SCREENED BY LOW OPAQUE WALLS OR HEDGES. WALLS AND HEDGES ARE TO BE BETWEEN 2' AND 4' HIGH.		
LANDSCAPE		<ul style="list-style-type: none"> ONE STREET TREE PER 25' OF STREET FRONTAGE TREE SIZE AND SPECIES ARE TO BE SELECTED FROM THE CITY OF DAYTONA BEACH APPROVED STREET TREE LIST MAJOR STREETS (ISB, MILK, ORANGE, MIMB, KEECH & BELLVUE): STREET TREES WILL BE PROVIDED AS PART OF THE PUBLIC SPACE INFRASTRUCTURE SECONDARY AND MINOR STREETS: STREET TREES WILL BE PROVIDED BY THE OWNER/DEVELOPER 		
		FIRST FLOOR, RETAIL, OFFICE AND PUBLIC USE MUST BE 60% OR MORE TRANSPARENT. THIS STANDARD WILL BE ACHIEVED FOR "BIG BOX" STORES, MOVIE THEATERS AND OTHER LARGE OPAQUE SURFACES BY WRAPPING THE SOLID SURFACES WITH LINER BUILDINGS THAT MEET THE 60% TRANSPARENCY STANDARD.		
ARCHITECTURE	STREET WALL	BUILDING FENESTRATION PATTERN AND CORNICE LINE MAY NOT BE CONTINUOUS FOR MORE THAN 75'		
	BALCONIES	<ul style="list-style-type: none"> 75% OR MORE OF RESIDENTIAL UNITS ON THE 2ND FLOOR MUST HAVE COVERED BALCONIES 50% OR MORE OF THE RESIDENTIAL UNITS ON THE 3RD FLOOR MUST HAVE COVERED BALCONIES BALCONIES MUST BE 6' DEEP OR MORE 		
DETAILS	<ul style="list-style-type: none"> BUILDING DORMERS WITH GLAZING MUST BE IN AN OCCUPIED ROOM OR SERVE AS LIGHT WELLS COVERED ARCADES ARE ENCOURAGED ON THE GROUND FLOOR 	<ul style="list-style-type: none"> PRIVATE PORCHES AND ENTRIES ARE TO BE RAISED 2' TO 3' ABOVE GRADE PORCHES MUST BE 8' DEEP OR MORE 		

[Zone 3] ACTIVITY CENTER MIXED-USE



[ZONE 4] NEIGHBORHOOD TRANSITION MIXED-USE (NT)



The Neighborhood Transition (NT) is established to accommodate a mix of residential, institutional, and small scale, low intensity, and “convenience” retail and service uses that provide goods/services to residents of the immediate neighborhood and the larger Daytona Beach area.

The neighborhood transition zones typically occur where a residential area abuts a primary or secondary street. There are a number of historic churches in these areas and future church renovations and expansions will be allowed that are consistent with the development standards. Residential uses are encouraged on the upper floors of nonresidential establishments. The district is subject to standards intended to ensure development is consistent with the neighborhood scale and compatible with surrounding uses.

Architectural Standards:

- 4.1 Facade modules shall have an emphasis on vertical proportions with dim. no less than 20' wide and no more than 35' in width.
- 4.2 All glazing at the ground level should be clear glass or a type that allows a view into the interior spaces.
- 4.3 Buildings with ground floor commercial uses shall have a 10' or higher ceiling.
- 4.4 Arcades and colonnades shall have a 10' minimum deep clear dimension.
- 4.5 Arcade and colonnade column bay proportions shall emphasize the vertical dimension. Vertical dim. will be equal to or more than the horiz. dim.
- 4.6 Materials & design elements that are encouraged:
 - masonry, stucco, cementitious siding & metal ext. finishes
 - arcades & awnings
 - porches, balconies, breezeways, pergolas, sun shades
 - transom windows above doors
- 4.7 Prohibited materials & elements:
 - mirror glass, vinyl siding, window AC units



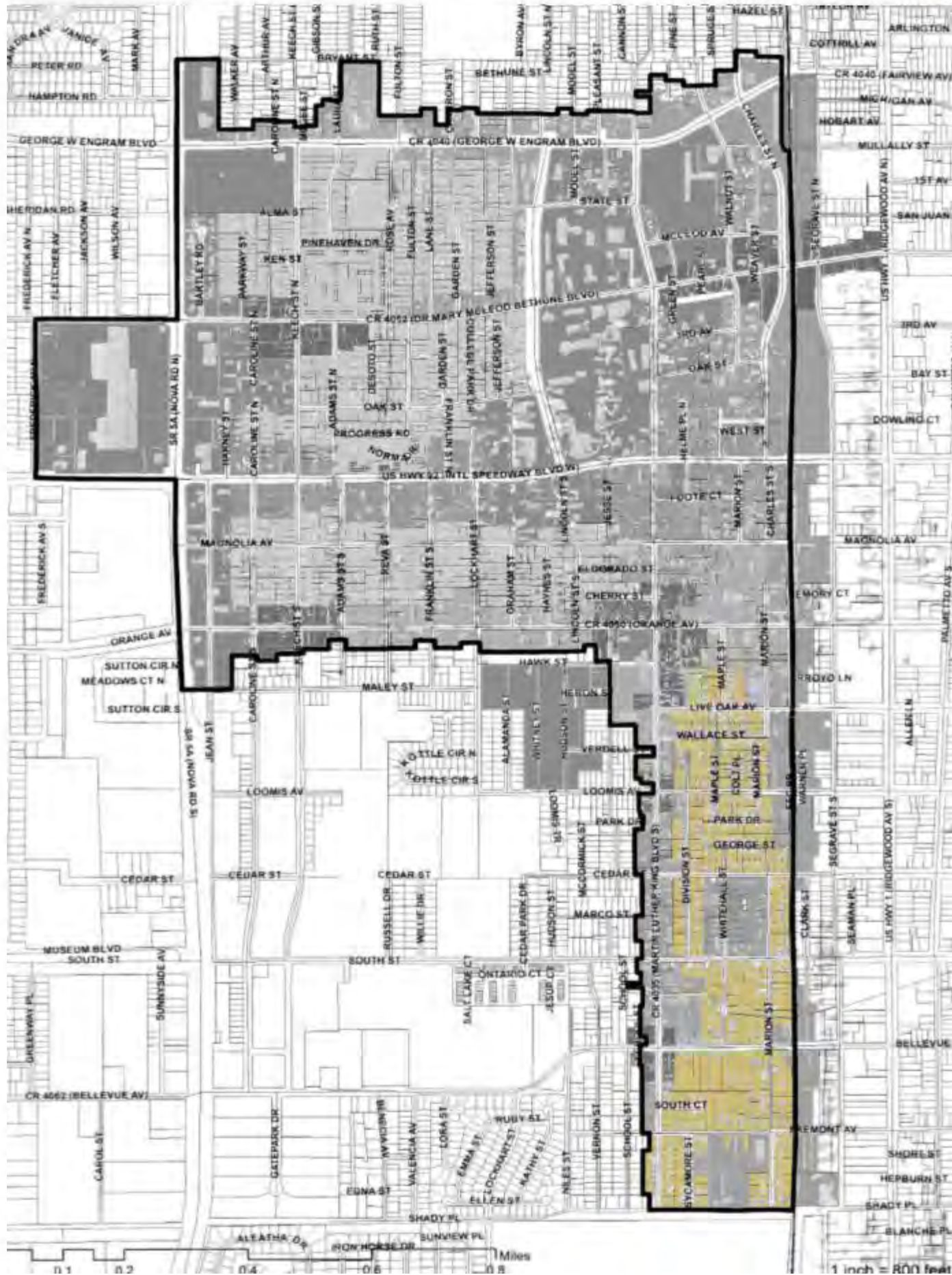
The intended scale & character of Building Design in Zone 4 (NT).

Development Standards & Design Guidelines: Design Guideline Illustration



This illustration shows a series of small commercial buildings with party wall construction. New linear retail buildings are to have varied facade modules that vary in width between 20' & 35'. This building massing is permitted in Zones 2,3,4, and 7.

[ZONE 6] RESIDENTIAL PRESERVATION / HISTORIC INFILL (RP-HI)



The Residential Preservation Historic Infill (RP-HI) zoning district is established to preserve the historic residential use and small scale characteristics of the area south of Magnolia Ave. and east of MLK Blvd. , as shown on the district map (left). Development in the district should not include uses of a size that is out of scale with a residential neighborhood. A mix of residential building types is encouraged. This area has a significant number of contributing “wood frame vernacular” historic structures. New construction and substantial revisions/additions to a facade facing a public street or park must comply with the architectural design guidelines in Zone 6. The district encourages a mix of market rate, work force and affordable housing. Accessory structures, Carriage Houses, out-buildings and small infill residential projects are encouraged in this area.

Architectural Standards:

6.1 Massing, density and character of new housing will be consistent with the existing historic neighborhood. Refer to the Illustrations, Examples and Dimensional Standards in this section for more detailed requirements.

6.2 Materials & design elements that are required:

- masonry & stucco piers below raised front porches (raised porch slabs with expressed pilasters are permitted)
- wood or cementitious siding & stucco exterior wall finishes
- metal (galvalume) or shingle roof materials
- porches that cover 50% or more of the front facade
- double hung & casement windows with vertical proportions
- transom windows above doors

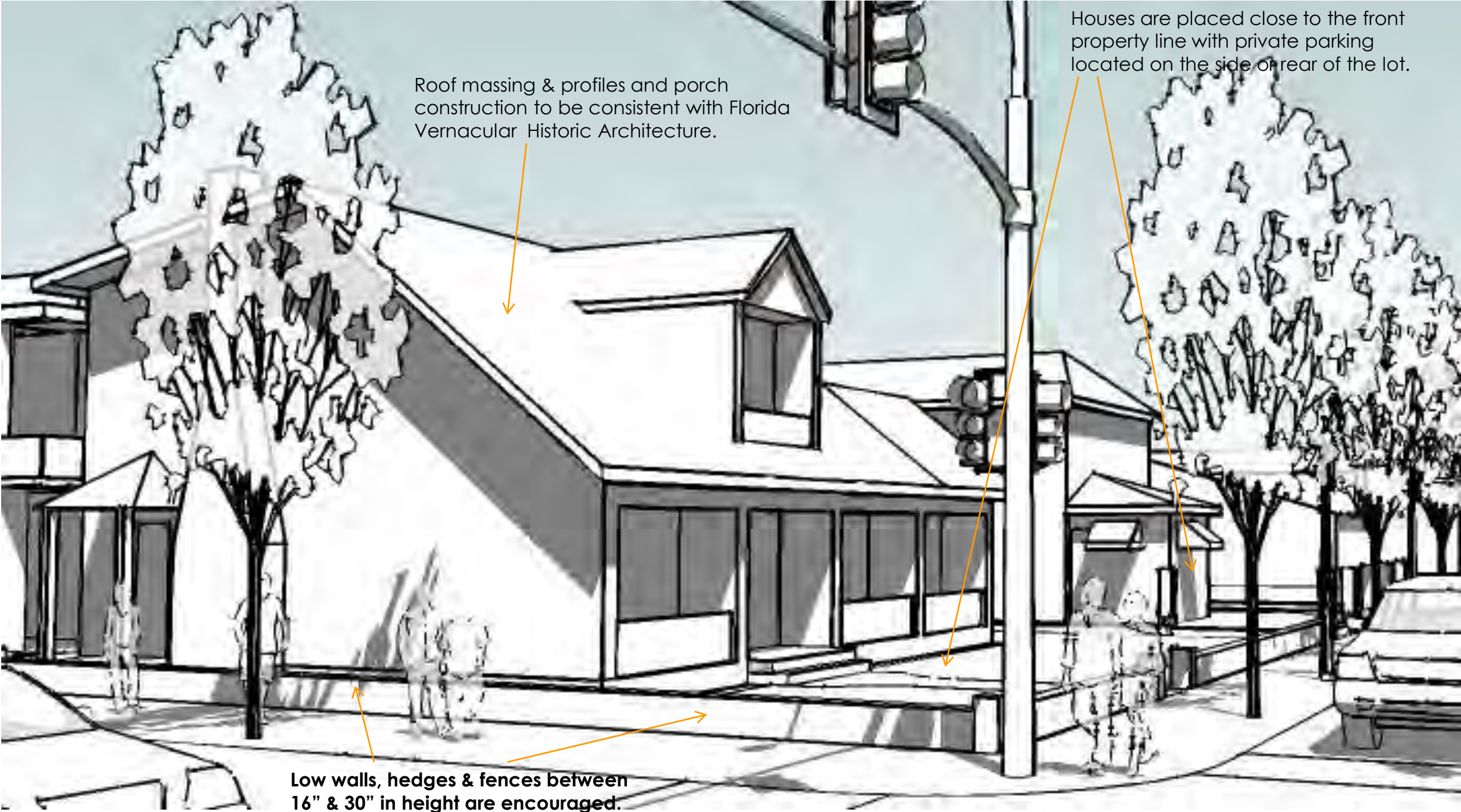
6.3 Prohibited materials & elements:

- EIFS (exterior insulation finish systems) at the ground floor
- mirror glass, vinyl siding, window AC units, drive thru facilities
- garage doors on the front facade – garages, carports & parking areas must be located on the side or rear of the principal building



The intended scale & character of Building Design in Zone 6 (RP-HI).

Development Standards & Design Guidelines: Design Guideline Illustration



This illustration shows the character of the historic residential and mixed use areas that are located in Midtown. New structures must be consistent with the existing development pattern. Front facades will be located between 10' & 20' from the front property line. This building pattern is permitted in Zones 2,4, 5, and 6.

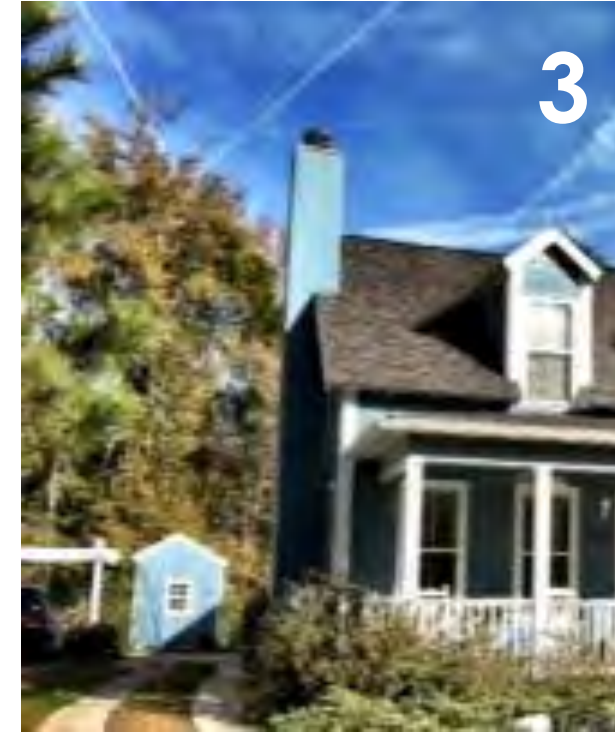
Development Standards & Design Guidelines: Design Guideline Examples



1. Covered porches are required on 50% or more of the front facade.
2. Garages & carports are to be located at the rear of the lot. Porte Cochere carports may be located on the side of dwellings.

These images include Florida Vernacular residential buildings; these design patterns are required in Zone 6: Residential Preservation / Historic Infill (RP-HI).

Development Standards & Design Guidelines: Design Guideline Examples



1. Attic stories are encouraged.
2. Dormers must have operable windows and be located in occupied spaces.
3. Fireplaces & chimneys are encouraged.
4. In private buildings, 1st floor elevations are to be raised 24" to 36" above grade.
5. Window & column bay proportions shall emphasize the vertical dimension. Vertical dimensions will be equal to or greater than the horizontal dimension.
6. Window & door shutters are to be operable & proportioned to match the opening.

These images show a series of Florida Vernacular residential buildings; these design patterns are required in Zone 6: Residential Preservation / Historic Infill (RP-HI).

[ZONE 6] RESIDENTIAL PRESERVATION / HISTORIC INFILL (RP-HI)

Dimensional Standards

STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
DENSITY	DENSITY, MAX. (DU/AC)	N/A	6/ACRE RESIDENTIAL BUILDING TYPES INCLUDE: DUPLEXES, TRIPLEXES, QUADRUPLICES AND COURTYARD HOUSING
	LOT SIZE, MINIMUM	N/A	N/A
LOT AREA	LOT WIDTH, MINIMUM	N/A	THE LESSER OF 50 FEET OR THE AVERAGE LOT WIDTH OF LOTS WITHIN 300 FEET ALONG THE SAME BLOCK FACE, PROVIDED THERE ARE 7 OR MORE LOTS WITHIN THE 300 FOOT AREA LOTS THAT ARE SERVED BY AN ALLEY MAY BE AS NARROW AS 40' WIDE
	BUILDING COVERAGE MAX. (%)	N/A	70%
IMPERVIOUS COVERAGE	N/A	85%	70%
FRONT	10' MINIMUM; 20' MAXIMUM		
SETBACKS	SIDE, MINIMUM	<ul style="list-style-type: none"> WHEN ABUTTING A STREET: 20 FT IF PARCEL IS LESS THAN 100 FT. IN WIDTH: 10 FT. NO SIDE YARD SHALL BE LESS THAN 5 FT. UNLESS IT IS PART OF A ZERO SIDE LOT LINE DEVELOPMENT 	<ul style="list-style-type: none"> WHEN ABUTTING A STREET: 5 FT NO SIDE YARD SHALL BE LESS THAN 5 FT. UNLESS IT IS PART OF A ZERO SIDE LOT LINE DEVELOPMENT
	REAR, MINIMUM	10'	10'
	CORNER, MINIMUM	15'	
ACCESSORY STRUCTURE	5' IF 600 S.F. IN SIZE OR SMALLER; OTHERWISE SAME AS PRINCIPAL USE		
GARAGE SETBACK, MINIMUM	40'		30' BEHIND FRONT FACADE
HEIGHT, MAXIMUM STORIES	2.5 STORIES		2.5 STORIES
ACCESSORY STRUCTURE	33% OF PRINCIPAL USE		33% OF PRINCIPAL USE

Dimensional Standards

STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
PARKING	N/A	<ul style="list-style-type: none"> 1 SPACE/STUDIO, AND 1 OR 2 BEDROOM UNITS 2 SPACES/3 BEDROOM UNITS 3 SPACES/4 BEDROOM UNITS 1 SPACE PER SLEEPING UNIT/TRANSIENT HOUSING ON STREET PARKING MAY BE COUNTED WITHIN A 350' RADIUS 	<ul style="list-style-type: none"> 4 SPACES - MAXIMUM IN GARAGES CARPORTS AND DRIVES
	ALL OFFSTREET PARKING IS TO BE LOCATED AT THE MIDDLE, REAR & SIDE OF THE LOT. IT IS TO BE PLACED BEHIND AND TO THE SIDE OF EXISTING AND NEW BUILDINGS. PARKING LOTS ARE NOT ALLOWED IN FRONT OF BUILDINGS. PARKING THAT ABUTS THE FRONT AND SIDE STREET PROPERTY LINES ARE TO BE SCREENED BY LOW OPAQUE WALLS OR HEDGES. WALLS AND HEDGES ARE TO BE BETWEEN 2' AND 4' HIGH.		
LANDSCAPE	STREET TREES <ul style="list-style-type: none"> ONE STREET TREE PER 25' OF STREET FRONTAGE TREE SIZE AND SPECIES ARE TO BE SELECTED FROM THE CITY OF DAYTONA BEACH APPROVED STREET TREE LIST MAJOR STREETS: STREET TREES WILL BE PROVIDED AS PART OF THE PUBLIC SPACE INFRASTRUCTURE SECONDARY AND MINOR STREETS: STREET TREES WILL BE PROVIDED BY THE OWNER/DEVELOPER COMMUNITY GARDENS FRUIT AND VEGETABLE GARDENS ARE ALLOWED AND ENCOURAGED IN ALL SIDE AND REAR YARDS.		
	FIRST FLOOR MUST HAVE AN ENTRY PORCH OR COVERED STOOP		
ARCHITECTURE	WINDOW PROPORTIONS ARE TO BE VERTICAL AND THEY ARE TO RELATE TO THE PORCH COLUMN BAYS		
	<ul style="list-style-type: none"> 75% OR MORE OF RESIDENTIAL UNITS ON THE 2ND FLOOR MUST HAVE COVERED BALCONIES IN MULTIFAMILY RESIDENTIAL BALCONIES MUST BE 6' DEEP OR MORE 		
DETAILS	<ul style="list-style-type: none"> BUILDING DORMERS AND CUPOLAS WITH GLAZING MUST BE IN AN OCCUPIED ROOM OR SERVE AS LIGHT WELLS PRIVATE PORCHES AND ENTRIES ARE TO BE RAISED 2' TO 3' ABOVE GRADE PORCHES MUST BE 8' DEEP OR MORE COVERED PORCHES WITH 6" OR 8" SQUARE COLUMNS ARE REQUIRED ON THE FRONT FACADE OF ALL HOUSES. THEY MUST BE 50% OR MORE OF THE FACADE LENGTH.		

[ZONE 6] RESIDENTIAL PRESERVATION / HISTORIC INFILL (RP-HI)

Future Development Pattern



[1] HOWARD THURMAN HOUSE
 [2] COMMUNITY GARDEN

Additional Notes

Future Development Pattern - Building Massing



[ZONE 7] ECO INDUSTRIAL USE (ECI)

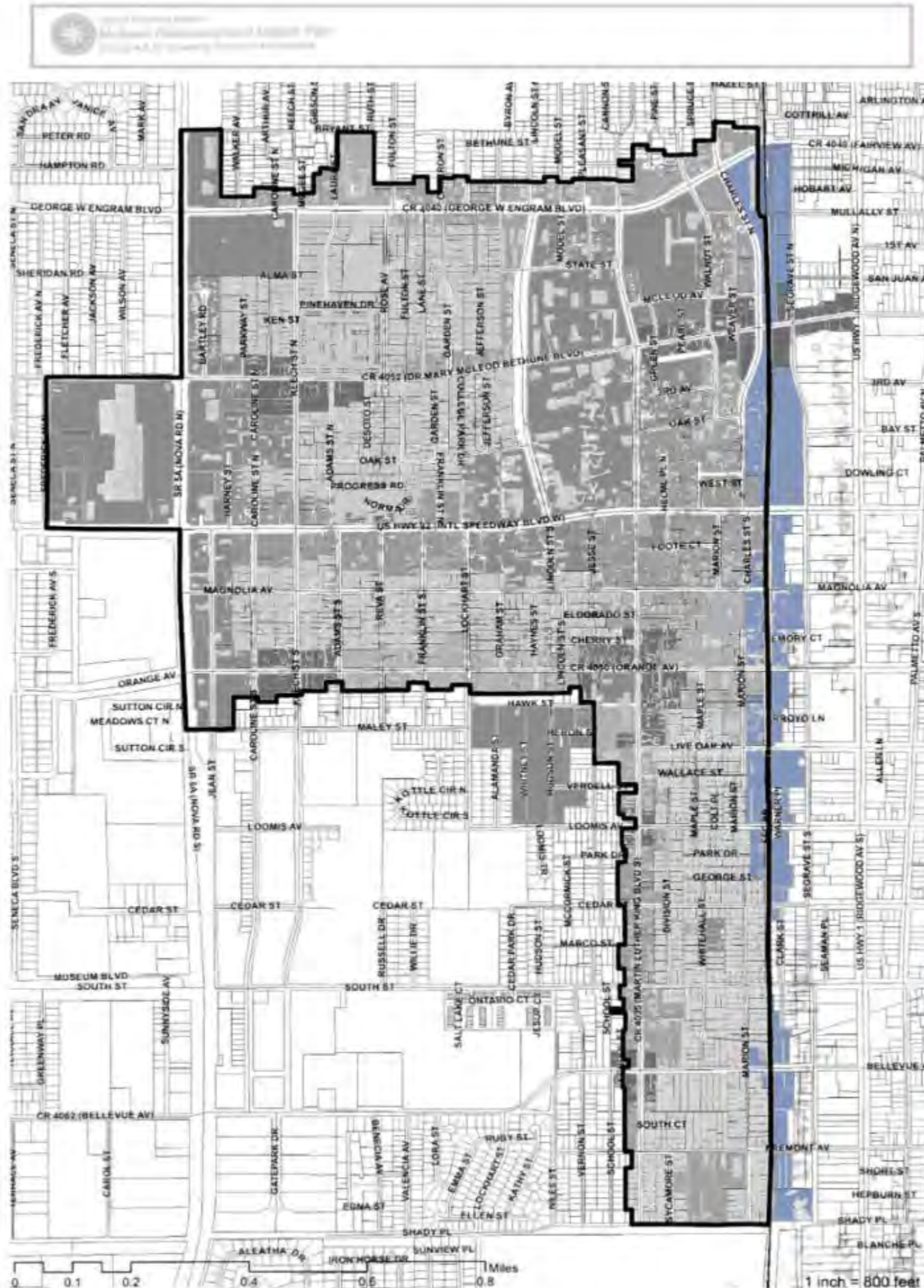
The Eco-Industrial (ECI) zoning district is centered on the railroad track along the eastern edge of Midtown, as shown on the district map (left). There are a number of existing light industrial uses in the area that will remain. The district encourages new business and light industry that is based on ecologically sensitive models of business operation and production.

Due to its central location in the city, there are opportunities for “green” manufacturing, storage and distribution. Art studios, laboratories and research uses associated with the numerous colleges and universities in the area are also encouraged.

Architectural Standards:

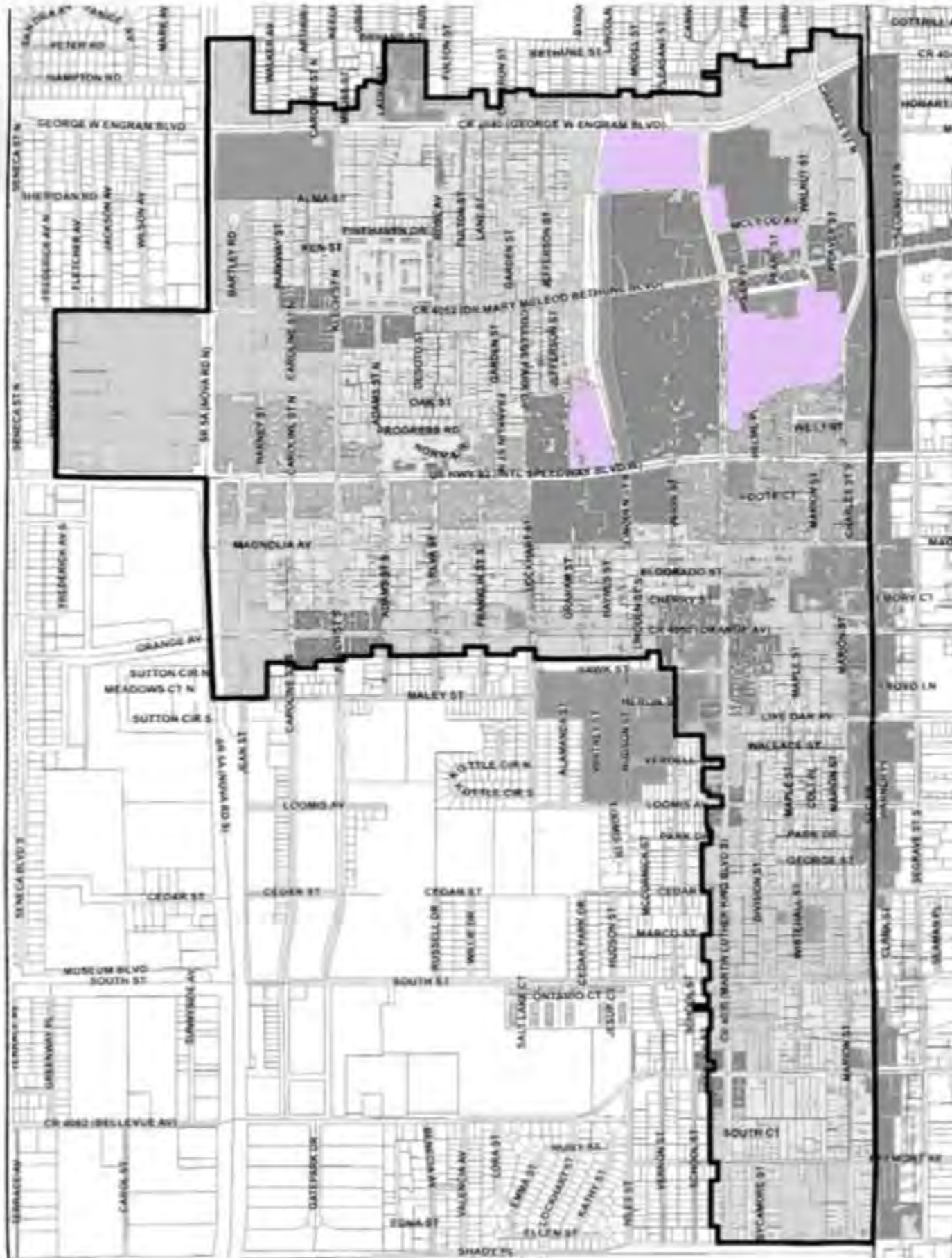
7.1 Service yards and outdoor storage areas must be completely enclosed or screened from view.

** Also refer to the C.O.D.B. Land Development Code for applicable environmental standards regarding industrial operations, noise levels, particulate levels, materials storage and disposal.*



The intended scale & character of Building Design in Zone 7 (ECI).

[ZONE 8] UNIVERSITY TRANSITION (UT)



The University Transition (UT) zoning is established to accommodate a mix of residential, institutional/religious and small scale, low intensity, and “convenience” retail and service uses that support and compliment Bethune Cookman University.

Residential uses are encouraged on the upper floors of nonresidential establishments. The district also includes areas that are appropriate for university expansion. The district includes standards intended to ensure that development is consistent with the scale and quality of the BCU campus and compatible with surrounding uses. The BCU and Midtown master plans create an opportunity to connect the campus and community as they both grow and change. The expansion of campus facilities and related campus services to the east will also establish an improved market for the shops and stores located on Mary McCloud Bethune Blvd (east of campus).

Architectural Standards:

- 8.1 Facade modules shall have an emphasis on vertical proportions with dim. no less than 20' wide and no more than 50' in width.
- 8.2 All glazing at the ground level should be clear glass (for retail uses) that allows a view into the interior.
- 8.3 Buildings with ground floor commercial uses shall have a 12' or higher ceiling.
- 8.4 Arcades and colonnades shall have a 10' minimum deep clear dimension.
- 8.5 Arcade and colonnade column bay proportions shall emphasize the vertical dimension. Vertical dim. will be equal to or more than the horiz. dim.
- 8.6 Materials & design elements that are encouraged:
 - masonry & stucco exterior finishes
 - arcades & awnings
 - balconies, galleries & roof terraces
 - porches, breezeways, pergolas, sun shades
 - transom windows above doors
 - space for outdoor dining
- 8.7 Prohibited materials & elements:
 - EIFS (exterior insulation finish systems) at the ground floor
 - mirror glass, vinyl siding, window AC units, drive thru facilities

Both traditional & contemporary architecture are appropriate in the UT district.



The intended scale & character of Building Design in Zone 8 (UT).

Development Standards & Design Guidelines: Design Guideline Illustration



Architectural massing & roof profiles relate to the scale & character of the BCU campus

Vertical "Facade Modules" & scale breaks respond to the urban setting. Facade modules shall be no less than 20' and no more than 50' in width.

Balconies are required on upper levels. Balconies must be a minimum of 6' deep. "Double French Door balconies" may be 2' deep. Refer to Dimensional Standards on next page for balcony location % requirements.

Ground floor retail & office uses located in corners at major intersections

This illustration shows a mixed-use residential building that is appropriate for the Midtown Town Center and the areas adjacent to the BCU campus in the University Transition Zone; it is permitted in Zones 1, 3 and 8.

[ZONE 8] UNIVERSITY TRANSITION (UT)

Dimensional Standards

STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
DENSITY DENSITY, MAX. (DU/AC)	60 / ACRE	40/ACRE	N/A
LOT AREA LOT SIZE, MINIMUM	N/A	N/A	24 ft. frontage
LOT AREA LOT SIZE, MAXIMUM	3.5 ACRES	2 ACRES	N/A
LOT AREA BUILDING COVERAGE MAX. (%)	80% * *If lot is less than 10,000, buildings may cover 90% of the lot.	75% * *If lot is less than 10,000, buildings may cover 90% of the lot.	60%
LOT AREA IMPERVIOUS COVERAGE	90%	85%	70%
SETBACKS FRONT	10' MINIMUM; 20' MAXIMUM		
SETBACKS SIDE, MINIMUM	<ul style="list-style-type: none"> WHEN ABUTTING A STREET: 20 FT IF PARCEL IS LESS THAN 100 FT. IN WIDTH: 10 FT. NO SIDE YARD SHALL BE LESS THAN 5 FT. UNLESS IT IS PART OF A ZERO SIDE LOT LINE DEVELOPMENT 	<ul style="list-style-type: none"> WHEN ABUTTING A STREET: 10 FT NO SIDE YARD SHALL BE LESS THAN 5 FT. UNLESS IT IS PART OF A ZERO SIDE LOT LINE DEVELOPMENT 	
SETBACKS REAR, MINIMUM	15'	10'	
SETBACKS CORNER, MINIMUM	15'		
SETBACKS ACCESSORY STRUCTURE	5' IF 600 S.F. IN SIZE OR SMALLER; OTHERWISE SAME AS PRINCIPAL USE		
SETBACKS GARAGE SETBACK, MINIMUM	20'	20' BEHIND FRONT FAÇADE OR MORE	
MASSING HEIGHT, MAXIMUM STORIES	2 STORY MINIMUM (EXCEPT FOR LARGE ASSEMBLY USES) 5 STOREY MAXIMUM		2.5 STORIES
MASSING ACCESSORY STRUCTURE	N/A	33% OF PRINCIPAL USE	

Dimensional Standards

STANDARD	MIXED-USE & OTHER AREAS	MULTI-FAMILY RESIDENTIAL	SINGLE FAMILY RESIDENTIAL
PARKING	<ul style="list-style-type: none"> RESTAURANTS AND CAFES: 1 SPACE/100 S.F. AREA GENERAL RETAIL: 1 SPACE/250 S.F. AREA OFFICE: 1 SPACE/400 S.F. AREA ONSTREET PUBLIC PARKING MAY BE COUNTED WITHIN A 1000' RADIUS PARKING SHARING MAY BE USED FOR COMPATIBLE USES THAT HAVE DIFFERENT PARKING SCHEDULES 	<ul style="list-style-type: none"> 1 SPACE/STUDIO, AND 1 OR 2 BEDROOM UNITS 2 SPACES/3 BEDROOM UNITS 3 SPACES/4 BEDROOM UNITS 1 SPACE PER SLEEPING UNIT/TRANSIENT HOUSING ON STREET PARKING MAY BE COUNTED WITHIN A 350' RADIUS 	<ul style="list-style-type: none"> 4 SPACES - MAXIMUM IN GARAGES CARPORTS AND DRIVES
PARKING	<p>ALL OFFSTREET PARKING IS TO BE LOCATED AT THE MIDDLE, REAR & SIDE OF THE LOT. IT IS TO BE PLACED BEHIND AND TO THE SIDE OF EXISTING AND NEW BUILDINGS. PARKING LOTS ARE NOT ALLOWED IN FRONT OF BUILDINGS. PARKING THAT ABUTS THE FRONT AND SIDE STREET PROPERTY LINES ARE TO BE SCREENED BY LOW OPAQUE WALLS OR HEDGES. WALLS AND HEDGES ARE TO BE BETWEEN 2' AND 4' HIGH.</p>		
LANDSCAPE	<ul style="list-style-type: none"> ONE STREET TREE PER 25' OF STREET FRONTAGE TREE SIZE AND SPECIES ARE TO BE SELECTED FROM THE CITY OF DAYTONA BEACH APPROVED STREET TREE LIST MAJOR STREETS: STREET TREES WILL BE PROVIDED AS PART OF THE PUBLIC SPACE INFRASTRUCTURE SECONDARY AND MINOR STREETS: STREET TREES WILL BE PROVIDED BY THE OWNER/DEVELOPER 		
STREET WALL	FIRST FLOOR, RETAIL, OFFICE AND PUBLIC USE MUST BE 60% OR MORE TRANSPARENT	ENTRY STOOPS, PORCHES AND FRONT DOORS MUST FACE A PUBLIC STREET, LANE OR COURT.	ENTRY STOOPS, PORCHES AND FRONT DOORS MUST FACE THE PRIMARY STREET.
ARCHITECTURE	BUILDING MASSING & FENESTRATION PATTERN AND CORNICE LINE MAY NOT BE CONTINUOUS FOR MORE THAN 50' REFER TO ILLUSTRATED EXAMPLES		
BALCONIES	<ul style="list-style-type: none"> 75% OR MORE OF RESIDENTIAL UNITS ON THE 2ND FLOOR MUST HAVE COVERED BALCONIES 50% OR MORE OF THE RESIDENTIAL UNITS ON THE 3RD FLOOR MUST HAVE COVERED BALCONIES BALCONIES MUST BE 6' DEEP OR MORE 		
DETAILS	<ul style="list-style-type: none"> BUILDING DORMERS, LIGHT MONITERS & CUPOLAS WITH GLAZING MUST BE IN AN OCCUPIED ROOM OR SERVE AS LIGHT WELLS COVERED ARCADES AND / OR AWNINGS ARE ENCOURAGED ON THE GROUND FLOOR 	<ul style="list-style-type: none"> PRIVATE PORCHES AND ENTRIES ARE TO BE RAISED 2' TO 3' ABOVE GRADE Where on grade accessibility is necessary, a 30" to 48" high garden wall may be used in place of a raised stoop or porch. PORCHES MUST BE 8' DEEP OR MORE 	

REQUIRED “GREEN” STANDARDS:

In addition to reducing heat gain and energy costs, these design strategies respond to the local climate and establish an architectural identity that is appropriate for the Daytona Beach Midtown District.

- 1. Windows, storefronts and curtain walls on the south and west sides of buildings are to be recessed (30” min.) from the face of the building OR have awnings, canopies, Bahama shutters or other shading devices. The shading devices must shade 75% or more of windows and 50% or more of storefronts and curtain walls at 2 pm on June 21st.**
- 2. The EPA’s (Environmental Protection Agency) mitigation strategies for the “heat island effect”, such as high solar reflectance index (SRI) or “cool roofs” with high albedo roof surfaces shall be followed. Roofs will have lighter tones with surface reflectance values of .65* or higher and emittance of .86*.
*(ASHRAE Energy Standards)**
- 3. Low-flow plumbing fixtures and water conserving toilets are required for new construction and major renovations. (major renovations are defined as...construction that costs 33% or more of the total value of the building and property)**

RECOMMENDED “GREEN” STANDARDS AND INCENTIVES:

- 1. If a “green roof” is used for 50% or more of the roof area, the applicant will receive a 20% density bonus.**
- 2. If a water collection system with cisterns is installed that supplies water for toilet flushing, the applicant will receive a 10% density bonus.**
- 3. If on site reclaimed water is used for irrigation, the applicant will receive a 10% density bonus.**
- 4. If a water collection system that supplies water to both toilets and irrigation system is used, the applicant will receive a 20% density bonus**
- 5. If bioswales, rain gardens and pervious paving are used, the applicant will receive a 10% to 20% density bonus – depending on the stormwater quantities that are diverted from the city’s storm sewer infrastructure.**

Development Standards & Design Guidelines: 5.3 Lighting, Signage & Color

LIGHTING STANDARDS:

1. **All walkway and parking lot fixtures are to have “light cutoffs” so that light is directed down to the ground.**
2. **Pedestrian walkways are to have light bollards approximately 30” high or pedestrian scaled fixtures between 12’ and 18’ high.**
3. **High pressure sodium lamps are not permitted.**

***Also refer to the C.O.D.B. Land Development Code for additional standards regarding footcandles and safety requirements.**

SIGNAGE STANDARDS:

1. **Signs should compliment and relate to other elements in the building such as awnings, canopies, shutters and artwork.**
2. **Illuminate signs with an external light source that uplights or downlights the sign. Large internally illuminated signs are not permitted. (exception – Zone 3). Individual letters, numbers and logos that are internally illuminated are allowed in Zones 1, 2, 3, 7 and 8.**
3. **Hanging signs in arcades, banners and “artwork” signs are encouraged.**
4. **In general, auto oriented “highway” signage – ie, over scaled internally lit bright plastic boxes, light boxes with changeable letters, low quality plastic and temporary signs are not permitted.**

*** Also refer to the C.O.D.B. Land Development Code for additional standards regarding signage locations and dimensions.**

COLOR STANDARDS:

1. **Midtown has a very rich color palette. It includes the warm tones of the terra cotta brick masonry on the BCU campus; the vibrant colors of yellow, blue and green wood frame houses in the historic residential area and the landmark white and red churches located along Martin Luther King Blvd. Colors for new buildings should compliment and relate to these existing color patterns.**
2. **In general, use light and medium tones for the main body of the building with brighter colors for accents and special elements.**
3. **Roof surfaces should be light to reduce heat gain into the building.**

Street lights must be directed down to the ground with “light cutoffs” as shown in this example.



Banners on street lights & wells at street trees are encouraged.



Example of a unique sign that is related to the history of the area.

Redevelopment Proposals: 4.11 Implementation Strategies - Immediate Actions: 2012 to 2015

1. Adopt the plan
2. It is critical that information is shared and various public departments, private initiatives, existing businesses and residents work together to achieve the plan. To that end, a Project Manager / Coordinator position must be established. This person will coordinate all Midtown related redevelopment activities & initiatives and should have experience in planning/design and/or real estate development. The Project Manager would coordinate the following:
 - a) All public works projects / improvements; b) public funding initiatives; c) medium and large scale private development projects; d) packaging and marketing of targeted redevelopment sites; e) coordination with existing businesses and property owners; f) coordination with Bethune Cookman University expansion; g) code enforcement and condemnation, and h) any other major development activities associated with the Midtown CRA. The Project Manager would work under the direction of the City of Daytona Beach Planning / Redevelopment Department and periodically update the Midtown Board.
3. Create a funding and grant initiative data base.
4. Develop a series of projects that will be funded by Community Block Grants from the U.S Department of Housing and Urban Development.
5. Create a Black Heritage Walk and Florida African American Museum & Research Center funding initiative.
6. Establish partnerships with national, regional and local community banks, lenders, investors and foundations.
7. Develop business start up, entrepreneur and expansion incentives.
8. Establish and market an improved clear, logical and efficient permitting and entitlement review / approval process for renovations, expansion and new development. Development approvals would be based on the proposed Land Development Regulations and Development Standards in the adopted Midtown Master Plan.
9. Create a Midtown real estate website for public and private land that includes cost, characteristics and all permitting / entitlement limits and requirements. Each parcel would have links with the Master Plan with illustrations of possible development scenarios.
10. Initiate public and private property acquisitions, exchanges and consolidations to achieve the adopted Master Plan.
11. Encourage and promote “grassroots” community improvement organizations and initiatives. An example is the *Community Garden Stewardship Program*. Through healthy food / healthy neighborhood programs – various churches, community groups, nonprofit organizations, BCU student groups and others would participate in the Midtown Community Garden. The garden would be located on public property and each group would have a raised garden plot that they would plant, maintain and harvest. A public market would be established during certain times of the year to sell produce and flowers. There would also be “friendly competitions” between the various community groups. Food that is not sold would be donated to neighborhood residents and families.
12. Establish a demolition and salvage program for abandoned and condemned buildings.
13. Establish a green renovation program which includes an energy audit and offer low-interest loans for renovations and building improvements that reduce energy and water consumption.
14. Create an incentive program for renovating and modifying through adaptive reuse the existing historic buildings and the contributing structures in Zone 6 – Residential Preservation / Historic Infill District.

Redevelopment Proposals: 4.11.1 Implementation Strategies

Mid-Range Actions: 2016 to 2025

1. Finalize the land acquisition for the new Town Center Plaza / Square and Dickerson Park Expansion.
2. On primary streets – place all overhead electrical below ground; complete drainage, sidewalk, street light & on street parking improvements.
3. Significant private sector development is started. The plan is “fleshed out” through incremental small and medium scale development.
4. The Lincoln Greenway and Thurman Gardens are completed.

Long Range Actions: 2026 to 2035

1. The Town Center and all 5 Neighborhood Centers are completed.
2. Significant private sector development is completed. The plan is “fleshed out” through incremental small, medium and large scale development.

Regarding commercial real estate development in underserved communities....

“Ultimately, it is retailers in partnership with developers that drive development. Therefore, it is important for the public officials and community leaders to understand the “cycle” of economic development in general and the retail development process specifically in order to have realistic expectations for development and to work productively with developers to meet the development objectives of the community. For example, even if all the identified challenges and issues are addressed, in most cases there will still be a necessary progression to the retail development of underserved markets - often beginning with fast food restaurants and drug stores, progressing to grocery stores and discount apparel and shoes and eventually ending with specialty retailers and higher priced concepts.”

Credit: Development in Underserved Retail Markets

A summary of a discussion of the challenges and recommendations for improving the process of retail development in underserved, urban markets

Co-written and Edited by:

Cynthia Stewart, Director, Local Government Relations & Alexis Morris, Manager, Community Investment
International Council of Shopping Centers Business for Social Responsibility

“Economic Development is encouraged by certainty and clear development standards that establish a consistent level of quality. A property owner, developer or business does not want to invest in an area where they have no idea of what could happen on either side of their project.”

Craig Huffman AIA, Architect & Professor
FL A & M University

Implementation Case Study: Gaines Street Corridor, Tallahassee, FL

The Private Sector

The city has invested over \$40 million in the old industrial corridor located between the FAMU and FSU campuses. The funds were primarily generated through a 1 penny sales tax that was targeted towards positive economic development. Gaines Street was one of a number of projects that used **improved urban design / development standards** combined with necessary infrastructure improvements. This strategy is leading to sustainable economic growth and development in the private sector and an improved tax base and quality of life for the targeted district and the city.



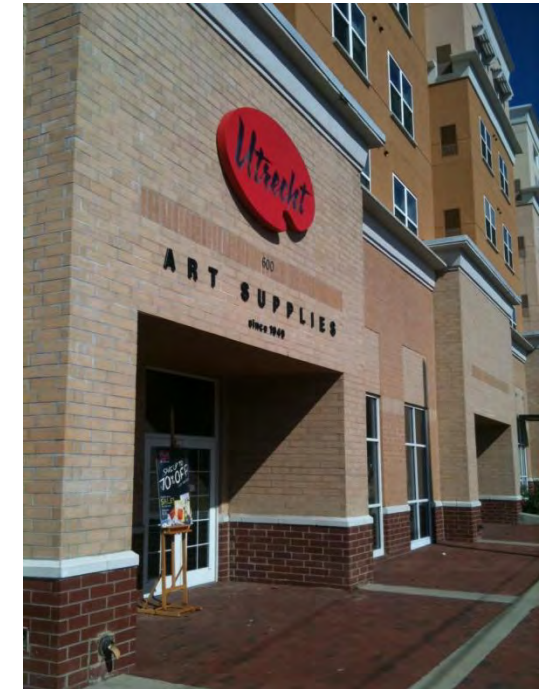
Public & educational institutions have been enthusiastic & supportive .



Local businesses have invested & expanded.



Local businesses have located in renovated existing buildings.



New mixed use buildings have been added that are guided by the Design Standards.

The Public Sector.....



Master Plans & Design Standards



Infrastructure



Themed Lighting & Signage



Public street parking



Enhanced pedestrian safety & an improved public environment



Design Standards: example – hedge screens at rear lot parking